



# San Gabriel Valley Council of Governments

## AGENDA AND NOTICE OF THE MEETING OF THE SGVCOG PUBLIC WORKS TECHNICAL ADVISORY COMMITTEE

Monday, September 16, 2019 – 12:00 PM

Monrovia Community Center: 119 W. Palm Ave., Monrovia, CA 91016

Chair  
**Rene Guerrero**  
City of Pomona

Vice Chair  
**David Liu**  
City of Diamond Bar

Immediate Past Chair  
**Phil Doudar**  
L.A. County DPW

### Members

*Arcadia*  
*Azusa*  
*Claremont*  
*Diamond Bar*  
*El Monte*  
*Glendora*  
*Irwindale*  
*Industry*  
*La Verne*  
*Monrovia*  
*Monterey Park*  
*Pomona*  
*San Dimas*  
*San Gabriel*  
*San Marino*  
*South El Monte*  
*South Pasadena*  
*Temple City*  
*West Covina*  
L.A. County DPW

Thank you for participating in today's meeting. The Public Works Technical Advisory Committee encourages public participation and invites you to comment on agenda items.

**MEETINGS:** *Regular Meetings of the Public Works Technical Advisory Committee are held on the third Monday of each month at 12:00 PM at the Monrovia Community Center – 119 W. Palm Ave., Monrovia, CA 91016.* The Public Works Technical Advisory Committee agenda packet is available at the San Gabriel Valley Council of Government's (SGVCOG) Office, 1000 South Fremont Avenue, Suite 10210, Alhambra, CA, and on the website, [www.sgvkog.org](http://www.sgvkog.org). Copies are available via email upon request ([sgv@sgvcog.org](mailto:sgv@sgvcog.org)). Documents distributed to a majority of the Committee after the posting will be available for review in the SGVCOG office and on the SGVCOG website. Your attendance at this public meeting may result in the recording of your voice.

**PUBLIC PARTICIPATION:** Your participation is welcomed and invited at all Public Works Technical Advisory Committee meetings. Time is reserved at each meeting for those who wish to address the Board. SGVCOG requests that persons addressing the Committee refrain from making personal, slanderous, profane, or disruptive remarks.

**TO ADDRESS THE PUBLIC WORKS TECHNICAL ADVISORY COMMITTEE:** At a regular meeting, the public may comment on any matter within the jurisdiction of the Committee during the public comment period and may also comment on any agenda item at the time it is discussed. At a special meeting, the public may only comment on items that are on the agenda. Members of the public wishing to speak are asked to complete a comment card or simply rise to be recognized when the Chair asks for public comments to speak. We ask that members of the public state their name for the record and keep their remarks brief. If several persons wish to address the Committee on a single item, the Chair may impose a time limit on individual remarks at the beginning of discussion. **The Public Works Technical Advisory Committee may not discuss or vote on items not on the agenda.**

**AGENDA ITEMS:** The Agenda contains the regular order of business of the Public Works Technical Advisory Committee. Items on the Agenda have generally been reviewed and investigated by the staff in advance of the meeting so that the Committee can be fully informed about a matter before making its decision.

**CONSENT CALENDAR:** Items listed on the Consent Calendar are considered to be routine and will be acted upon by one motion. There will be no separate discussion on these items unless a Committee member or citizen so requests. In this event, the item will be removed from the Consent Calendar and considered after the Consent Calendar. If you would like an item on the Consent Calendar discussed, simply tell Staff or a member of the Public Works Technical Advisory Committee.



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the SGVCOG office at (626) 457-1800. Notification 48 hours prior to the meeting will enable the SGVCOG to make reasonable arrangement to ensure accessibility to this meeting.



## **PRELIMINARY BUSINESS**

1. Call to Order
2. Pledge of Allegiance
3. Roll Call
4. Public Comment (*If necessary, the Chair may place reasonable time limits on all public comments*)

## **CONSENT CALENDAR** (*It is anticipated that the Committee may take action on the following matters*)

5. Review Public Works TAC Meeting Minutes: 07/15/2019 (**Page 1**)  
*Recommended Action: Review and approve.*

## **PRESENTATIONS**

6. San Gabriel Valley Regional Bikeshare Program – Caitlin Sims, Principal Management Analyst, SGVCOG (**Page 5**)  
*Recommended Action: For information.*
7. Statewide Energy Efficiency Collaborative Resources for Local Governments – Leif Christiansen, Statewide Coordinator, Local Government Commission (**Page 15**)  
*Recommended Action: For information.*

## **ACTION ITEMS** (*It is anticipated that the Committee may take action on the following matters*)

8. I-605 Corridor Aesthetics Master Plan – Mark Christoffels, Chief Engineer, SGVCOG (**Page 41**)  
*Recommended Action: Recommend the SGVCOG Governing Board to approve the I-605 Corridor Aesthetics Master Plan.*

## **DISCUSSION ITEMS** (*It is anticipated that the Committee may take action on the following matters*)

9. LACMTA Technical Advisory Committee Representative Appointment – Alexander Fung, Management Analyst, SGVCOG (**Page 79**)  
*Recommended Action: Discuss and provide direction to staff.*

## **UPDATE ITEMS** (*It is anticipated that the Committee may take action on the following matters*)

10. Upcoming Changes to Southern California Edison's Local Government Partnership Programs – Alexander Fung, Management Analyst, SGVCOG  
*Recommended Action: For information.*

## **STAFF ANNOUNCEMENTS**

11. San Gabriel Valley Energy Wise Partnership City Staff Energy Work Group Meeting  
*Recommended Action: For information.*
12. Next Committee Meeting  
*Recommended Action: For information.*

**ANNOUNCEMENTS**

**ADJOURN**





**SGVCOG Public Works TAC Meeting Minutes**

Date: July 15, 2019  
 Time: 12:00 P.M.  
 Location: Monrovia Community Center  
 119 West Palm Avenue, Monrovia, CA 91016

**PRELIMINARY BUSINESS**

1. Call to Order  
The meeting was called to order by R. Guerrero at 12:10pm.
2. Pledge of Allegiance  
R. Guerrero led the Public Works TAC in the Pledge of Allegiance.
3. Roll Call

**Members Present**

R. Delgadillo; Azusa  
 D. Liu; Diamond Bar  
 S. Mendez; El Monte  
 R. Ramos; Irwindale  
 D. Keesey, A. Ciotti; La Verne  
 A. Tachiki; Monrovia  
 R. Guerrero; Pomona  
 K. Patel, S. Barragan; San Dimas  
 A. Ross, M. Adhami; L.A. County DPW

**Members Absent**

Arcadia  
 Glendora  
 Industry  
 Monterey Park  
 South El Monte  
 South Pasadena  
 Temple City

**SGVCOG Staff**

K. Ward  
 A. Fung

**Guests**

Joshua Torres, SCE  
 Jeannette Soriano, SCE  
 Diego Cadena, WRE  
 Fred Alamolhoda, LAE  
 Jared Dever, SGVMVCD  
 Shawn Arianna, Geo-Advantec  
 Violette Sedagat, Geo-Advantec  
 Bahman Janka, City of Pasadena

4. Public Comment  
There was no public comment.

**CONSENT CALENDAR**

*The committee voted on each of the items within the consent calendar separately.*

5. Review Public Works TAC Meeting Minutes: 06/17/2019  
**There was a motion to approve the committee meeting minutes from June 17, 2019. (M/S: Azusa/San Dimas)**

**[Motion Passed]**

Ayes	Azusa, El Monte, Irwindale, La Verne, Monrovia, Pomona, San Dimas, Los Angeles County DPW
Noes	

Abstain	
Absent	Arcadia, Diamond Bar, Glendora, Industry, Monterey Park, South El Monte, South Pasadena, Temple City

6. Election of Chair and Vice Chair for FY 2019-2020

**There was a motion to elect Rene Guerrero as the Chair and David Liu as the Vice Chair of the Public Works TAC for FY 2019-2020. (M/S: La Verne/San Dimas)**

**[Motion Passed]**

Ayes	Azusa, Diamond Bar, El Monte, Irwindale, La Verne, Monrovia, Pomona, San Dimas, Los Angeles County DPW
Noes	
Abstain	
Absent	Arcadia, Glendora, Industry, Monterey Park, South El Monte, South Pasadena, Temple City

**PRESENTATIONS**

7. Southern California Edison EV-Ready Communities Paper

Joshua Paul Torres, Southern California Edison’s Senior Policy Advisor, provided a presentation on a paper that Southern California Edison (SCE) published to assist local governments in preparing for the increased adoption of electric vehicle (EV) Technology by residents and businesses in Southern California. Mr. Torres mentioned that the paper was developed based on the feedback from the Southern California Association of Governments and stakeholders in SCE’s service territories. During the presentation, Mr. Torres stressed that having a EV-readiness plan can pay off dividends since a lot of information can be copied-and-pasted on grant applications for EV chargers.

Questions/Discussions:

- A member of the public inquired about rebate opportunities for multifamily dwellings. Mr. Torres responded that the rebate opportunities that are listed in the presentation are available to all customers to encourage the adoption of EVs in the community.
- A committee member inquired about the cost of charging a vehicle in comparison to the cost of fueling a vehicle that utilizes gasoline. Mr. Torres responded that the cost of charging a vehicle is less than \$1.50/gallon in comparison.
- Another committee member inquired about whether the adoption of an EV-readiness plan is required to apply for grants. Mr. Torres responded that cities do not have to develop and adopt an EV-readiness plan in order to apply for grants, but it is useful to have a plan in place.
- A committee member inquired about whether homeowners are required to receive a permit from the local jurisdiction. Mr. Torres responded that most jurisdictions do require residents to apply for a permit if they want level 2 chargers; however, some level 2 chargers may not be required for hybrid vehicles.
- A member of the public inquired if Southern California Edison is working with the car manufacturers to maintain the hybrid market in the industry. Mr. Torres responded that the company has partnerships with OEMs and recognizes that hybrids are good transitions for residents that have “range anxieties.” Mr. Torres

also mentioned that it is cheaper to manufacture all-electric vehicles and that some all-electric vehicles still have their complementary hybrid models in the market.

- A committee member inquired about the process to streamline permitting on SCE's side. Mr. Torres responded that the company is addressing this matter and mentioned that it can be difficult to streamline the process given that local jurisdictions have many different processes; however, the company is working with cities to develop better processes to figure out the streamlining process more quickly.
- Another committee member inquired about the method to pay for charging EVs in public. Mr. Torres, as well as other committee members, mentioned that methods of payment depends on the charger's location; however, most businesses offer payments through mobile applications.

## DISCUSSION ITEMS

### 8. Region-wide Swimming Pool Ordinance

Jared Dever, San Gabriel Valley Mosquito and Vector Control District's (SGVMVCD) District Manager, provided a presentation on the regionwide swimming pool ordinance. Mr. Dever provided a recap of previous meeting's presentation on this topic and explained the severity of leaving a swimming pool unmaintained for a long period of time. Mr. Dever reported that around 95% of the residents began to maintain their swimming pools again after receiving the first notice. For the remaining 5% of residents that are not complying with maintaining their swimming pools, Mr. Dever recommended cities to adopt an ordinance stating that out-of-service swimming pools are in violation of city code. In the most recent aerial surveillance patrol, around 9,000 swimming pools were flagged as unmaintained swimming pools in the San Gabriel Valley.

#### Questions/Discussions:

- A committee member inquired about whether the proposed ordinance is used to allow SGVMVCD to assist cities to maintain the swimming pools. Mr. Dever responded that a list of unmaintained swimming pools was sent to the cities a week and a half ago. The listed swimming pools are flagged as health hazards and the SGVMVCD requires the local jurisdictions' assistance in order to enter the properties to fix the pools.
- Another committee member inquired about the recipients of the list of flagged swimming pools. Mr. Dever responded that the list was sent to SGVMVCD's Board of Trustees. For those that did not receive the list, Mr. Dever will collaborate with COG staff to share the list with committee members.
- A member of the public inquired whether the mosquito problem is only a phenomenon in the San Gabriel Valley region. Mr. Dever replied that mosquitoes using unmaintained swimming pools as breeding grounds is a global problem.
- Another member of the public inquired about the methods used to flag swimming pools with problems. Mr. Dever responded that photos are taken during a scheduled aerial surveillance using a fixed-wing aircraft. Images are then separated by color and quality of water using advanced GIS software that automatically geocodes the locations for the agency.
- A committee member inquired about the removal of the fees and costs section in the original draft of the swimming pool ordinance. Mr. Dever responded that SGVMVCD's legal team recommended to move the fees and costs section to be under the administrative citations section.

**There was a motion for the Public Works TAC to recommend the SGVCOG City Managers’ Steering Committee to review the SGVMVCD regionwide swimming pool ordinance. (M/S: Pomona/San Dimas)**

**[Motion Passed]**

Ayes	Azusa, Diamond Bar, El Monte, Irwindale, La Verne, Monrovia, Pomona, San Dimas, Los Angeles County DPW
Noes	
Abstain	
Absent	Arcadia, Glendora, Industry, Monterey Park, South El Monte, South Pasadena, Temple City

**ACTION ITEMS**

There were no action items reviewed or discussed at this meeting.

**UPDATE ITEMS**

There were no update items reviewed or discussed at this meeting.

**COMMITTEE MEMBER ITEMS**

There were no committee member items reviewed or discussed at this meeting.

**STAFF ANNOUNCEMENTS**

**9. City Staff Energy Work Group Meeting**

Alexander Fung from SGVCOG provided an announcement on the upcoming City Staff Energy Work Group meeting that will be held on Tuesday, July 16, 2019 at 1:30pm at the Monrovia Community Center. Mr. Fung mentioned that all committee members are invited to join the work group meeting to receive updates from utility companies and share best practices in implementing energy efficiency projects in the San Gabriel Valley region. Additionally, the work group meeting will include presentations on the new 2019 SoCalREN Public Agency Programs and renewable natural gas.

**10. Upcoming Presentations and Meeting Dates**

Alexander Fung from SGVCOG provided an announcement on the committee’s future meeting dates. The Public Works TAC will not meet for the month of August and will resume regular meetings beginning in September. The next meeting is scheduled for Monday, September 16, 2019 at 12:00pm at the Monrovia Community Center.

**ADJOURN**

The meeting adjourned at 12:57pm.



DATE: September 16, 2019

TO: SGVCOG Public Works Technical Advisory Committee

FROM: Marisa Creter, Executive Director

RE: **SAN GABRIEL VALLEY REGIONAL BIKESHARE PROGRAM**

## **RECOMMENDED ACTION**

For information only.

## **BACKGROUND**

In August 2017, the San Gabriel Valley Council of Governments (SGVCOG) was awarded funding by the California Transportation Commission (CTC)'s Greenhouse Gas Reduction Fund Grant Program in the amount of \$4.55 million to form a regional pilot bike share program in the San Gabriel Valley. The SGVCOG's goals for the Program are to decrease drive alone trips and increase the share of bicycling trips within the San Gabriel Valley, promote and encourage bicycling as a safe and sustainable mode of transportation for short local trips, including one-way trips, and expand low- cost transportation options in the region.

This funding will support the implementation of a regional e-bike share program, deploying a minimum of 840 bikes in at least 15 participating cities. The program will be available to cities at no cost. Following a competitive procurement and technical evaluation process completed by SGVCOG and City staff, Gotcha Media Holdings was selected to deploy the 840 e-bikes throughout the region. Gotcha also offers scooters, trikes and ride share services.

SGVCOG staff is currently working with the Gotcha team and participating cities to implement the regional pilot program. Several key tasks have been completed, including the following:

- **Development of template ordinance:** This template can be used by cities to establish the shared mobility pilot program within their city. It will provide the framework and establish parameters for the pilot bike share program.
- **Refinement of station types:** Based on site visits and discussions with the bike share working group, COG staff is recommending cities utilize Gotcha stations and virtual hubs (geo-fenced) that are indicated by mats/designated return zones.

SGVCOG staff is also working on the following activities:

- **Station Siting:** Cities evaluating potential station sites in conjunction with the Gotcha team, identifying locations that would offer the most convenient location for residents to acquire a bike in high traffic areas such as universities, large employers, and shopping/dining hubs.
- **Launch Events:** SGVCOG staff is working with cities to select appropriate events to launch the bike share program.

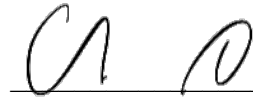
The first launch event will be held in conjunction with the 626 Golden Streets: Streets and Treats Open Streets event to be held in El Monte and South El Monte on Saturday, October 26, 2019.

The overall timeline is as follows:

- **October 2019:** 626 Golden Streets & Treats (El Monte, South El Monte)
- **February 2020:** Last chance for cities to participate
- **April 2020:** 626 Golden Streets: Heart of the Foothills (Pomona, Claremont, La Verne)
- **June 2020:** All bikes deployed

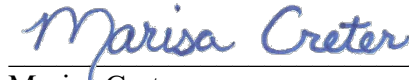
SGVCOG Principal Management Analyst, Caitlin Sims, will provide an overview of the program and current status at the meeting.

Prepared by:



Caitlin Sims  
Principal Management Analyst

Approved by:



Marisa Creter  
Executive Director

## ATTACHMENTS

Attachment A – Presentation on San Gabriel Valley Regional Bikeshare Program



## San Gabriel Valley Bike Share Program

September 2019

### Program Overview

- In August 2017, the San Gabriel Valley Council of Governments (SGVCOG) was awarded by the California Transportation Commission (CTC) a Greenhouse Gas Reduction Fund Grant in the amount of \$4.55 million to form a bike share program in the San Gabriel Valley.
- This grant will fund at least 15 participating cities, which includes a minimum of 840 bikes and stations throughout the region

## Program Goals

The SGVCOG's goals for the bike share program are to:

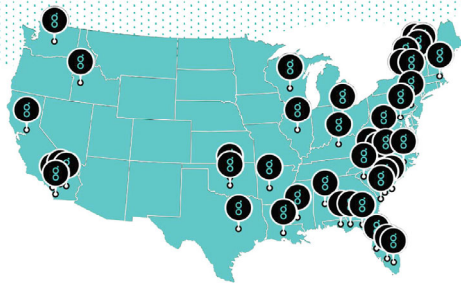
1. Decrease drive alone trips and increase the share of bicycling trips within the San Gabriel Valley.
2. Promote and encourage bicycling as a safe and sustainable mode of transportation for short local trips, including one-way trips.
3. Expand low-cost transportation options.

## Request for Proposal (RFP) Process

- The bike share equipment, infrastructure, ongoing operations/maintenance and program launch are at no cost to participating cities.
- At least 420 bicycles in the fleet must serve communities that are identified as Disadvantaged Communities
- Selection Process:
  - 6 Proposals were received.
  - The 3 highest ranked firms were interviewed.
  - **Gotcha** was unanimously scored the highest by all TEC members after reviewing the written proposals and conducting interviews, which included testing of the bicycles.

## Gotcha Media Holdings

### WHO IS GOTCHA?



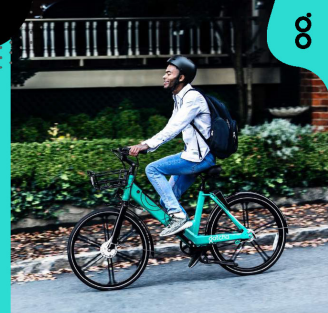
**50+**  
MOBILITY SYSTEMS

**5,000+**  
MOBILITY ASSETS

**1.5 MM+**  
ANNUAL TRIPS

STRICTLY PRIVATE  
& CONFIDENTIAL

03



E-BIKE FEATURES

- 100% electric
- Magnesium alloy wheels
- Automatic safety lights
- 350-watt motor
- Carbon fiber belt drive
- GPS enabled smart lock

08

## Gotcha Service Delivery

- Gotcha will provide:
  - Customer service to residents
  - Marketing to residents
  - Maintenance & charging of bikes
  - Rebalancing of bikes on a daily basis
  - Staffing needs including 1 regional manager, 4 community managers, 4 operations managers, 16 fleet team members, and 8 mobility ambassadors

## Program Overview

- The following cities have submitted formal letters of interest:
  - Arcadia
  - Claremont
  - County of Los Angeles (Unincorporated Communities in the San Gabriel Valley)
  - Covina
  - Duarte
  - El Monte
  - La Verne
  - Monterey Park
  - Pasadena
  - Pomona
  - San Dimas
  - South El Monte
  - South Pasadena

## Program Administration

- Template Bike Share **City Ordinance**
- City Memorandums of Agreement

## Bike Share Ordinance

- Purpose of ordinance:
  - Establishes pilot program and defines pilot program operator(s) & expectations
  - Types of devices allowed and disallowed
  - Locations devices allowed
  - Could specify process for future vendors
- Template has been prepared and distributed to cities

## City MOAs

SGVCOG	City
<ul style="list-style-type: none"> <li>• Manage coordination with Gotcha</li> </ul>	<ul style="list-style-type: none"> <li>• Establish Gotcha as sole bike share operator</li> </ul>
<ul style="list-style-type: none"> <li>• Monitor compliance with Gotcha performance indicators and assess penalties as necessary</li> </ul>	<ul style="list-style-type: none"> <li>• Help identify bike share stations</li> </ul>
<ul style="list-style-type: none"> <li>• Provide monthly program reports from Gotcha</li> </ul>	<ul style="list-style-type: none"> <li>• Designate point of contact</li> </ul>
<ul style="list-style-type: none"> <li>• Monitor and coordinate with Gotcha to ensure issues are being addressed</li> </ul>	<ul style="list-style-type: none"> <li>• Participate in program development &amp; coordination calls/meetings</li> </ul>
<ul style="list-style-type: none"> <li>• Coordinate marketing and launch activities with Gotcha</li> </ul>	<ul style="list-style-type: none"> <li>• Provide support as needed for launch</li> </ul>
	<ul style="list-style-type: none"> <li>• Notify SGVCOG of any proposed ordinances/regulations that could impact program</li> </ul>
	<ul style="list-style-type: none"> <li>• Report incidents of non-compliance and customer complaints to Gotcha and COG</li> </ul>

## Program Implementation

- Template Bike Share **City Ordinance**
- City Memorandums of Agreement

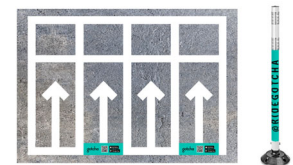
## Station Siting

- Cities are working with the Gotcha team to determine the best sites for hubs based on high traffic areas
- Stations
  - Require more space
  - Allow more orderly storage of bikes
- Mobility Mats
  - Require less space
  - Less orderly
  - Easiest to install and move



### PARKING HUBS

PAVEMENT MARKING TAPE, CUSTOM GRAPHICS AND BOLLARDS.

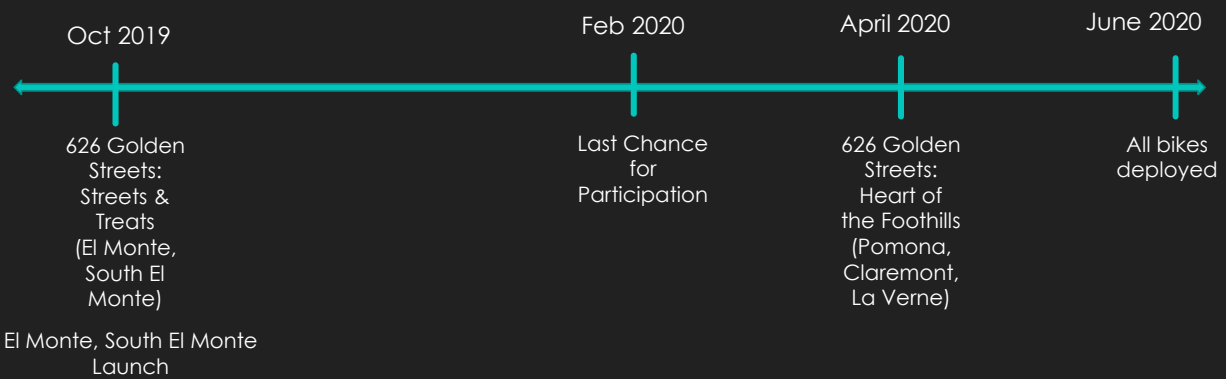




## Launch Events

- October 26, 2019: 626 Golden Streets: Streets and Treats
  - El Monte and South El Monte
- Other Launch Events?

## Timeline





# Questions/Discussion

DATE: September 16, 2019

TO: Public Works Technical Advisory Committee

FROM: Marisa Creter, Executive Director

RE: **STATEWIDE ENERGY EFFICIENCY COLLABORATIVE RESOURCES FOR LOCAL GOVERNMENTS**

## **RECOMMENDED ACTION**

For information only.


## **BACKGROUND**

The Statewide Energy Efficiency Collaborative (SEEC) is an alliance between California's four investor-owned utilities and three statewide nonprofit organizations. The Collaborative leverages the resources and diverse expertise of its implementing partners to meet evolving and ongoing local government needs to save energy, accelerate climate action, and reduce greenhouse gas emissions in California. SEEC is funded by California utility customers and administered by the Pacific Gas and Electric Company, San Diego Gas and Electric Company, Southern California Edison, and Southern California Gas Company under the auspices of the California Public Utilities Commission.

The Collaborative offers various no-cost resources for local governments to accelerate the implementation of cost-effective energy efficiency projects and policies, including:

- Best practices for local governments to implement energy efficiency measures;
- Networking opportunities for city staff and elected officials;
- Recognition opportunities and awards for local agencies that reduce greenhouse gas emissions and adopt policies that promote sustainability;
- Technical assistance to assist local agencies to overcome challenges to acquire energy data, implement energy efficiency measures, and expand staff capacity; and,
- Trainings and resources to provide city staff with an understanding of new trends and tools in the fields of climate change and energy.

Cities can learn more about SEEC and its no-cost resources at [www.californiaseec.org](http://www.californiaseec.org). SEEC's Statewide Local Government Energy Efficiency Coordinator, Leif Christiansen, will provide a brief presentation at this meeting.

Prepared by:   
Alexander P. Fung  
Management Analyst

Approved by:   
Marisa Creter  
Executive Director

**ATTACHMENTS**

Attachment A – Presentation on Statewide Energy Efficiency Collaborative

# Statewide Energy Efficiency Collaborative (SEEC) Overview

*Leif Christiansen,  
Energy Efficiency (EE) Coordinator*

*Public Works Technical Advisory Committee Meeting  
Monrovia Community Center  
September 16, 2019*

# Statewide Energy Efficiency Collaborative (SEEC)

3 NGOs, 4 Utilities, and 1 EE Coordinator!



CaliforniaSEEC.org

# What is Energy Efficiency (EE) and Why is it important?

Energy Conservation = Saving energy by doing with less or without

EE = Using less energy to provide the same level of service

## Key EE Benefits:

- ✓ Cost savings
- ✓ Improved operations
- ✓ Improved health and comfort
- ✓ Aligned with sustainability and climate change goals
- ✓ Fiscal responsibility



# Energy Efficiency: Big Impact, Little Attention

“Millions of small improvements over the past few decades—insulation, weather-stripping, better motors and engines, recovering waste heat, more-frugal chemical reactions, all the myriad fruits of careful engineering—add up to efficiency gains that now provide more global energy services than oil or any other fuel.”

- Amory Lovins, Cofounder, Rocky Mountain Institute



# Key Legislation

## AB 32 – California Global Warming Solutions Act of 2006

- Reduce greenhouse gas (GHG) emissions to 1990 levels by 2020, 40% below 1990 by 2030, and 80% below 1990 by 2050

## SB 350 – Clean Energy and Pollution Reduction Act

- Double EE achievements by 2030

## SB 100 – 100 Percent Clean Energy Act of 2018

- Carbon neutral electric grid by 2045

## EO B-55-18 To Achieve Carbon Neutrality

- Governor Executive Order which established new statewide goal to achieve carbon neutrality as soon as possible, and no later than 2045, and maintain net negative emissions thereafter



# Energy Efficiency Coordinator

**Goal: Accelerate CA local gov't implementation of cost-effective EE projects and policies that support achievement of local and state energy and climate goals**

## Key Strategies:

- Reducing local gov't energy usage footprint through increased implementation of EE upgrades and zero-net energy (ZNE) new construction projects
- Increasing local gov't EE achievements through leveraging energy/sustainability/climate initiatives
- Utilizing local gov't regulatory authority to increase adoption and implementation of local EE reach codes more stringent than Title 24 building code
- Diversifying local gov't funding sources to support on-going in-house EE capabilities and activities

# Key Resources



Energy  
Efficiency  
Coordinator

## wEEkly Update

A weekly email with EE news, legislative/regulatory updates, publications, job announcements and events

## EEcoordinator.info

Repository for news, case studies, best practices, and resources on EE and sustainability



## Local Government Commission

# Annual Statewide EE Forum

- Provides valuable opportunities for over 300 local governments and other participants to learn, share, and network with peers from across the state
- Highlights latest trends, replicable strategies, and tangible best practices for advancing climate and energy action in CA
- Provides many opportunities for peer-to-peer engagement through interactive sessions and networking activities




- First comprehensive online emissions management platform for local governments in the world

Statewide Energy Efficiency Collaborative  
AN ALLIANCE TO SUPPORT LOCAL GOVERNMENT

Jurisdiction: Mikeville ▾ Sign Out

Signed in successfully.

Home About SEEC



Welcome to the Statewide Energy Efficiency Collaborative (SEEC), Climate and Energy Management Suite. Within this set of tools you will be able to manage energy and greenhouse gas emissions at both the local government operation and community scales. The first two modules of the suite will allow you to perform an emissions inventory and forecast. Later in 2013, climate action planning and monitoring modules will be added. Select from the Government or Community track below to begin.

For more information on the tools contained in this application, download and read the user guides:

- Inventory Module
- Forecast Module

For technical support, please email [SEECApps@iclei.org](mailto:SEECApps@iclei.org)

#### Latest Inventories

Name	Year	Status	Records
TRANSPORT TEST	2013	In Progress	Records
2012 Inventory	2012	In Progress	Records
2012 Update	2012	In Progress	Records
4-july-test	2012	In Progress	Records
Test Inventory - JRK ICLEI	2012	In Progress	Records
VMT Forecast test	2011	In Progress	Records
2010 Inventory	2010	Complete	Records
Test 2010	2010	In Progress	Records
Just Upstream and Consumption	2008	In Progress	Records
test inventory e	2007	In Progress	Records



#### Government Track

Within this track you will find the resources you need to perform a Local Government Operations Protocol compliant greenhouse gas emissions inventory and forecast.



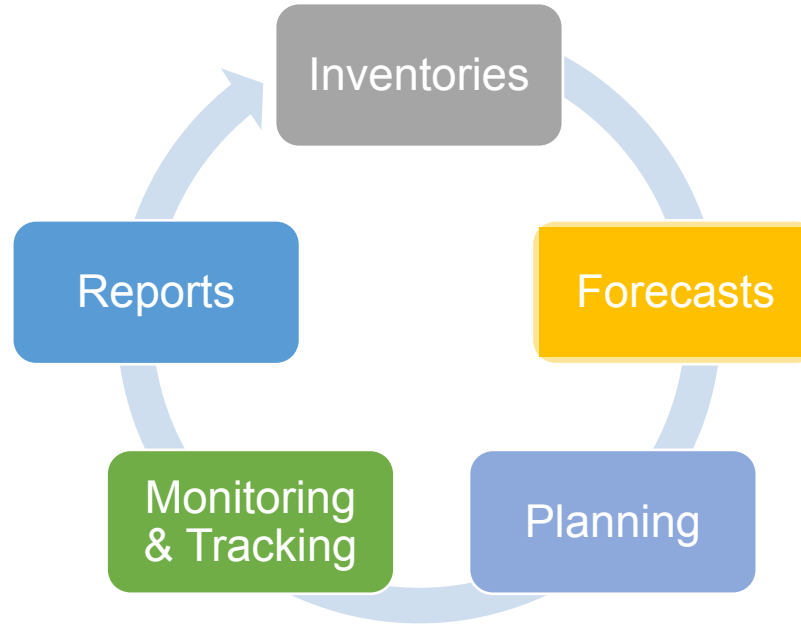
#### Community-Scale Track

Within this track you will find the resources you need to perform a US Community Protocol compliant greenhouse gas emissions inventory and forecast.

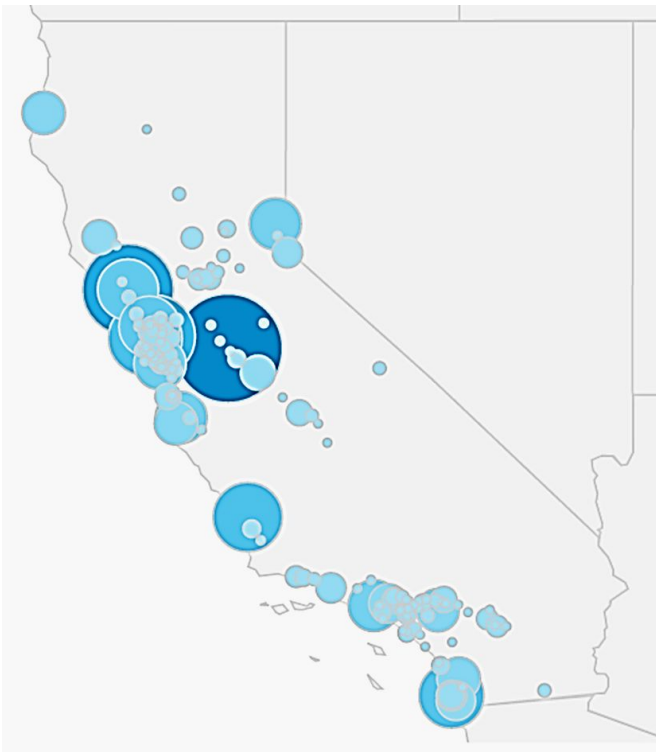
# Five Milestones for Climate Mitigation



# SEEC ClearPath's Five Modules



# SEEC ClearPath's Reach



477 US jurisdictions have collectively created 2,542 inventories in ClearPath.

216 California ClearPath jurisdictions with 1,056 total inventories

- 336 community-scale
- 227 local-government operations

Explored over 1,000 different BAU Projections

200 Reduction Scenarios with over 800 emissions-reduction strategy records



# Why Use SEEC ClearPath

- **Free access** – no consultant fees
- **User-friendly interface** with easy data entry and calculators
- Online, **Cloud-Based** Application
- **Collaboration** supported w/ multiple users per jurisdiction and multiple jurisdictions per user
- **Secure data storage** with daily backups
- **Remote support and training curriculum** with data collection templates
- **Streamline progress tracking** over time
- Automatic development **updates**
- Contribute to **statewide database**



# Recent Updates & Improvements

## Inventory Updates

- Consumption-based emissions calculators
- Fugitive emissions calculator
- Notation keys for GPC
- Grouping Tags & Indicator Reports
- Direct Entry for LGO Vehicle Fleet
- Gov Ops Employee Transit Use & Air Travel Calculators

## Planning Updates

- High level planning calculators
- Gov Operations New Measures: Low Power Computing, Reduced Air Travel, Cool Roofs, Discount Transit for Employees
- Community New Measures: Bike Share, Water Efficiency, Solar Thermal, Heat Pumps, Building Benchmarking, High Efficiency Transit

## Factor Sets

- Updated default emission factors and forecasting data from PG&E, SCE, SDG&E

## Reports

- Monitoring Reports
- GPC Reports

# Contact for ClearPath Demos and Climate/Energy Trainings

Calyn Hart  
Program Officer  
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(510) 844-0699

[icleiusa.org](http://icleiusa.org)

@ICLEI\_USA



# Beacon Program



Supporting California local governments



INSTITUTE FOR LOCAL GOVERNMENT

# About ILG

ILG is the official non-profit affiliate of:

- The League of California Cities (482)
- California State Association of Counties (58); and the
- California Special Districts Association (4,500)

ILG has been a proud member of SEEC since 2009, administering the Beacon program



# BEACON PROGRAM

## ILG Supports Busy Jurisdictions & Celebrates Sustainability

The Beacon Program provides a framework for cities and counties to [share best practices](#) and be [recognized](#) for voluntary efforts to [save energy](#), [reduce GHG emissions](#), and adopt policies and programs that promote sustainability.

ILG's flagship Beacon Program helps low-capacity governments document, implement and receive recognition for energy and climate action activities:

- Long-standing relationships help assess the current needs of local governments
- Successful framework is a model for engaging more disadvantaged communities
- 148 cities now participate

# Why join the Beacon Program?



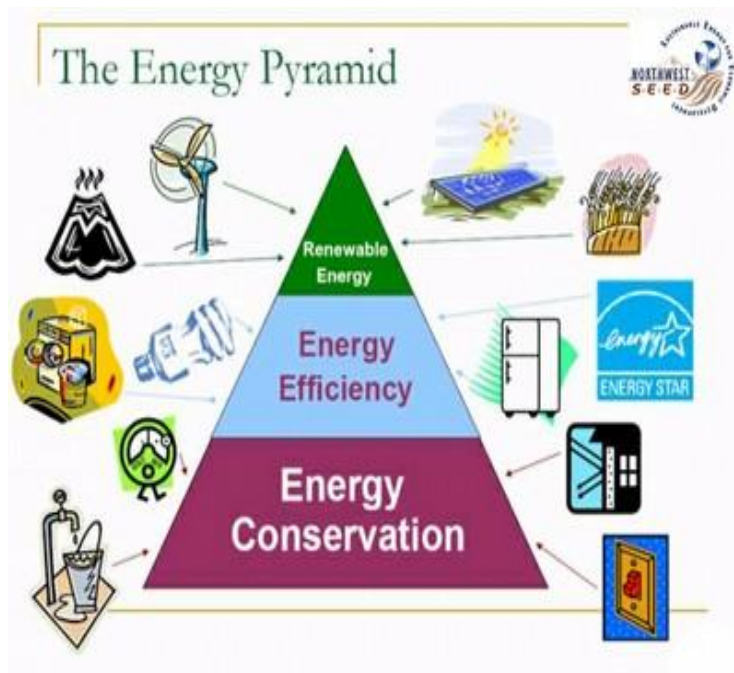
- Provides a framework to organize energy efficiency and sustainability efforts with little investment of time or money
- Provides a platform for sharing best practices and information
- Provides a statewide venue for achievements and recognition
- Integrates agency staff and elected officials on sustainability efforts and funding opportunities

# Energy Efficiency Best Practices

- Start With What You Have:  
Conduct Audits and Retrofits
- Fund For the Future: Create Your  
Own Funding Stream
- Promote Behavior Change:  
Involve Your Community
- Build Efficiently: Promote  
Progressive Policies



Supporting California local governments





# For EE and Other Best Practices:



Energy Efficiency  
& Conservation

Visit

[ca-ilg.org/sustainability-best-practices-framework](http://ca-ilg.org/sustainability-best-practices-framework)



Energy Efficiency  
& Conservation



Water &  
Wastewater  
Systems



Green Building



Waste Reduction  
& Recycling



Climate-Friendly  
Purchasing



Renewable Energy  
& Low-Carbon  
Fuels



Efficient  
Transportation



Land Use &  
Community  
Design



Open Space &  
Offsetting Carbon  
Emissions



Community &  
Individual Action

# Important Dates

**January-March** — Recruitment/Technical Support

**March 1** — Applications released

**June 1** — Application Deadline

**August** – Award Notifications

**October 17, 2019** — Awards Ceremony

**October-December** — Publicity/Case Stories

# Join In!



Kim Danko  
Senior Program Coordinator  
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# Questions!

Leif Christiansen

Statewide Local Government EE Coordinator

916-448-1198, x317

[lchristiansen@lgc.org](mailto:lchristiansen@lgc.org)

[CaliforniaSEEC.org](http://CaliforniaSEEC.org)

DATE: September 16, 2019

TO: Public Works Technical Advisory Committee

FROM: Marisa Creter, Executive Director

RE: **I-605 CORRIDOR AESTHETICS MASTER PLAN**

## **RECOMMENDED ACTIONS**

Recommend the SGVCOG Governing Board to approve the I-605 Corridor Aesthetics Master Plan.

## **SUMMARY**

With the development of several I-605 freeway projects (I-605 Corridor Improvement Project), a need arose to develop a Corridor Aesthetic Master Plan (CAMP) that could be applied to all on-going freeway projects funded by the Measure R and Measure M sales tax measures to include:

- Capacity and operational enhancements
- Additional general purpose lanes
- Ramp reconfigurations
- Toll lanes
- Increased signage

As a need to have a uniform approach to future highway improvements, the Gateway Cities Council of Governments (GCCOG) formed an Aesthetics Committee which was composed of the Los Angeles County Transportation Authority (LA Metro), the municipal members from the GCCOG, the San Gabriel Valley Council of Governments (SGVCOG), and the California Department of Transportation (Caltrans).

The Aesthetics Committee provided background information and community preferences for the overall design. The committee members worked in collaboration to develop a CAMP for the I-605 Project, and as a Committee approved the attached final document.

Each participating agency is now being requested to approve the CAMP with the understanding that these guidelines will be used on any future project constructed along the 605 Corridor from the 405 to the 210 freeways. For the SGVCOG, this means that any project from the 60 to the 210 freeways will need to be in conformance with this adopted document.

## **BACKGROUND**

The I-605 freeway has designated funding under both Measure R and Measure M to address deficiencies, in particular interchanges at the 5 and the 60 freeways as well as selected on- and off-ramps. A conceptual study of alternatives was completed in 2018 and an Environmental Impact

Report is currently being prepared to evaluate the alternatives and to select a preferred alternative. Under consideration on new freeway to freeway ramps, additional general travel lanes, and potential toll lanes. Many of these improvements would result in new retaining walls, bridge structures, ramps, soundwalls, lights and signage, and landscaping. In order to provide direction on the visual appearance of these improvements and to try to establish a corridor wide approach, it was agreed that a corridor aesthetics plan should be developed and adopted concurrently with the EIR approval. Metro procured a consultant contract to prepare this document and since most of this corridor is within their jurisdiction, the GCCOG took the lead in creating an Aesthetics Technical Advisory Committee. SGVCOG staff were included as a member of this Aesthetics Technical Advisory Committee and participated in the monthly meetings and reviewed and commented on the various draft reports. The SGVCOG worked closely with its member agencies that adjoined the 605 Corridor and would have an interest in the Aesthetics Master Plan. These agencies included the Cities of El Monte, South El Monte, Industry, Baldwin Park, and Irwindale and the County of Los Angeles.

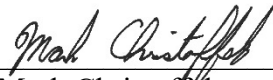
The I-605 Corridor Aesthetics Master Plan has now been completed and participating agencies have been asked to formally approve the plan. SGVCOG staff have reached out to our member agencies identified above and have received no objections to the SGVCOG approving this I-605 Corridor Aesthetics Master Plan.


## **BUDGET IMPACT**

This action will have no budget impacts to the SGVCOG. Any future projects that incorporate the aesthetic treatments identified in the plan will include those costs as part of the construction funding for those projects.

The I-605 CAMP is a general blueprint and does not prohibit different types of community identification and/or gateway monumentation that may be desired by cities on bridges or retaining walls within their jurisdiction. Generally, these types of additions to the standard freeway aesthetics are considered betterments and funded and maintained by the cities requesting them. During the project design phase, if a city desires these additional treatments, the city will need to enter into a separate agreement with the California Department of Transportation and fund the associated costs.

SGVCOG's Chief Engineer, Mark Christoffels, will provide a brief presentation on the I-605 Corridor Aesthetics Master Plan at this meeting.

Prepared by:   
Mark Christoffels  
Chief Engineer

Approved by:   
Marisa Creter  
Executive Director

**ATTACHMENTS**

Attachment A - I-605 Corridor Aesthetics Master Plan





# I - 605 CORRIDOR AESTHETIC MASTER PLAN



SAN GABRIEL RIVER FREEWAY



# I - 605 CORRIDOR AESTHETIC MASTER PLAN

*June 6, 2019*

## Aesthetic Committee Members

### Municipalities

City of Artesia	City of Lakewood
City of Baldwin Park	City of Long Beach
City of Bellflower	City of Lynwood
City of Cerritos	City of Norwalk
City of Compton	City of Pico Rivera
City of Downey	City of Santa Fe Springs
City of El Monte	City of South El Monte
City of Hawaiian Gardens	City of Whittier
City of Industry	Los Angeles County

### Gateway Cities, Council of Governments

Kekoa Anderson

### San Gabriel Valley, Council of Governments

Mark Christoffels

### Los Angeles Transportation Authority (LA Metro)

Ernesto Chavez, LA Metro	Lucy Olmos, LA Metro
Isidro Panuco, LA Metro	Catherine Saint, LA Metro

### California Department of Transportation (Caltrans)

Mehdi Salehinik, District 07	Quincy Wong, Bridge Architecture & Aesthetics
George Olguin, District 07	Isaac Tasabia, Bridge Architecture & Aesthetics
Glen Levstik, District 07	

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# 1.0 INTRODUCTION

This project is titled the *San Gabriel River Freeway (I-605) Corridor Aesthetics Masterplan* (hereafter referred to as the “I-605 CMP”).

## 1.1 Interstate 605 Aesthetics Committee and Approval Process

With the development of several I-605 freeway projects, (I-605 Corridor Improvement Project), a need arose to develop a Corridor Aesthetic Master Plan (CMP) that could be applied to all on-going freeway projects funded by the Measures R and Measure M sales tax measures to include:

- Capacity and operational enhancements
- Additional general purpose lanes
- Ramp reconfigurations
- Arterial intersection enhancements
- Increased signage
- Safety features

As a need to have a uniform approach to future highway improvements, the Gateway Cities COG formed an Aesthetics Committee which is composed of the Los Angeles County Transportation Authority (LA Metro), the municipal members from the Gateway Cities Council of Governments (GCCOG), the San Gabriel Valley Council of Governments (SGVCOG), and the California Department of Transportation (Caltrans).

The Aesthetics Committee provided background information and community preferences for the overall design. The committee members worked in collaboration to develop a CMP for the I-605 Project, and will provide aesthetics review, oversight, and approval as the design is refined in the design-build process.



The I-605 CMP serves to plan the visual quality of the I-605 transportation corridor. Therefore, all future transportation projects along the corridor shall have an accompanying document which identifies the appearance of planned physical elements, to include landscape and structures; which when fully implemented, shall reflect the committee’s overall vision for the I-605 corridor. The Design-Build Contractor (“Contractor”) will work closely with the I-605 Aesthetics Committee to ensure adherence to the I-605 CMP.

## 1.2 Context Sensitive Solutions

The I-605 CMP incorporates the Caltrans policy of “Context Sensitive Design Solutions” (CSS) which is “an approach to plan, design, construct, maintain, and operate its transportation system. These solutions use innovative and inclusive approaches that integrate and balance community, aesthetic, historic, and environmental values with transportation safety, maintenance, and performance goals. CSS are reached through a collaborative, interdisciplinary approach involving all stakeholders.” (Director’s Policy DP-22, Context Sensitive Solutions)

The purpose of CSS is to develop highway design elements and structures that are responsive to local values and concerns, through involvement of stakeholders, early in the planning stage. These interventions provide consistency and unity in the appearance and image of the route, through application of corridor aesthetic themes; while enhancing individual community identities. For the I-605 CMP, emphasis was

placed upon the history of the region, local geography, and the existing freeway structures and communities to produce the “Aesthetic Committee Master Plan Theme”. The Aesthetics Committee combined the diverse features into a comprehensive design that represents the totality of the I-605 corridor.

## 1.2.1 History of the Region - Historical & Cultural Considerations

The San Gabriel Valley is abundant with history of its early settlers and civilizations. From the aboriginal Tongva people, to the arrival of Spanish explorers and establishment of the Mission San Gabriel Arcangel. The Spanish (and later Mexican governments) also encouraged settlement by issuing large land grants called *ranchos*. Typically, the Ranchos encompassed the most valuable land near the Pacific coast, and inland near major rivers, which developed the foundation for California’s agricultural heritage and land surveying system.

These historic ranchos of early California were a primary influence on the “Aesthetic Committee” Master Plan Theme, and are reflected in many of the design elements.

One most notable rancho is Rancho Los Nietos. It was one of the first and largest land grants. The rancho was so vast, that today, the following communities



were located within its border: Anaheim, Artesia, Buena Park, Bolsa Chica, Cerritos, Cypress, Downey, Fullerton, Garden Grove, Huntington Beach, Lakewood, Long Beach, Los Alamitos, Naples, Norwalk, Santa Fe Springs, Seal Beach, Sunset Beach, and Whittier (*Wikipedia, Jan. 2019*).



Land development in the 20th and 21st century often followed the boundaries of the ranchos, and often retain the original name. Many of the cities continue to embrace this rancho theme through preservation and restoration of municipal buildings and street aesthetics.

# 1.0 INTRODUCTION

## 1.2.2 Local Geography - Environmental Considerations

Another primary influence on the “Aesthetic Committee Master Plan Theme” is the San Gabriel River. The San Gabriel Valley, situated between the San Gabriel Mountains to the North, and the Pacific Ocean to the South, derives its name from the San Gabriel River which runs parallel to the San Gabriel Freeway. Much of San Gabriel Valley’s histo-



ry originates with the river. From the early Tongva people relying on its waters for fish and surrounding game, to the Spanish and Mexican settlers using it for irrigation and ranching. Today, the San Gabriel River is still a major source of water for the local communities, and provides several recreation areas from the upper forks in the San Gabriel mountains, to the wetland areas near the Pacific. The significance of the river is reflected in the retaining wall design motif. The San Gabriel River flows through the San Gabriel Valley carrying its history and connecting the communities to their shared heritage.



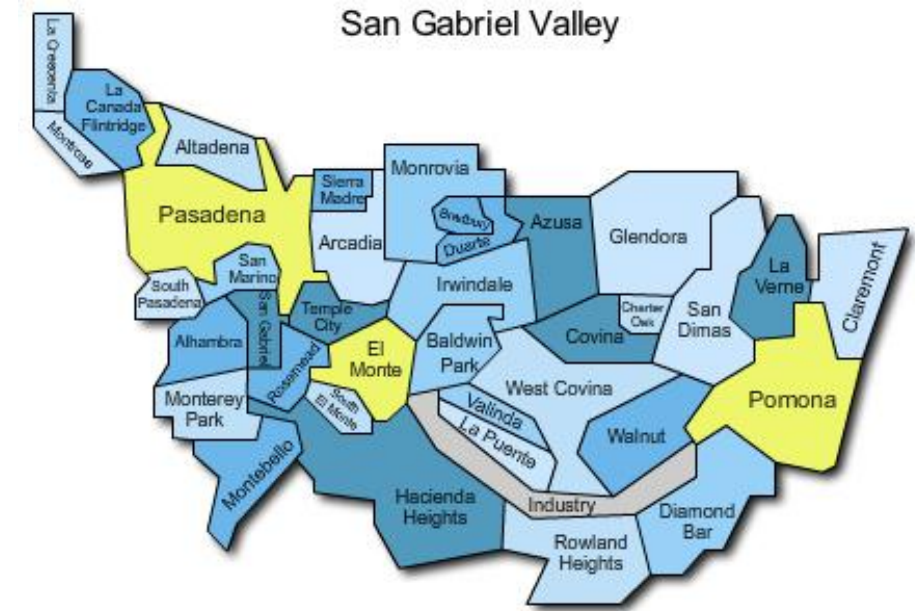
## 1.2.3 Existing Freeway Structures and Communities

Interstate 605, officially known as the San Gabriel Freeway, is a major Interstate Highway in the Greater Los Angeles area running approximately 27 miles from Duarte to Seal Beach.

Development of the I-605 Corridor theme was sensitive to the visual context of existing I-605 structures that will remain. Expressions of the “Aesthetic Committee Master Plan Theme” will be uniformly applied to the I-605 Project. Special consideration was directed at incorporating, or retrofitting existing structures with new aesthetic treatments to provide a unified aesthetic appearance. Examples will be matching the overall geometries of new and older structural components (columns etc.) at widened bridge locations.

I-605 aesthetics will blend influences of Interstate 5 where the highways cross. I-605 bridge structures which cross over the I-5 Corridor, will incorporate the same barrier, box girder, column, and abutment/slope paving as found on I-5 structures to maintain visual unity. Other design influences include the I-5 sound wall pilaster caps and sound wall block textures.

The aesthetics of visible structures will impact surrounding neighborhoods and communities along the I-605 Corridor. Most of the structures primarily seen are retaining walls, sound walls, and undercrossings. Their scale, shape, proportions, textures, color and materials not only need to reflect the corridor’s theme, but also integrate with adjacent community elements. Highly visible I-605 undercrossings will have opportunities for aesthetic enhancements such as; pilasters, gateway monuments, community identifiers, etc. to enhance the identity of each community.



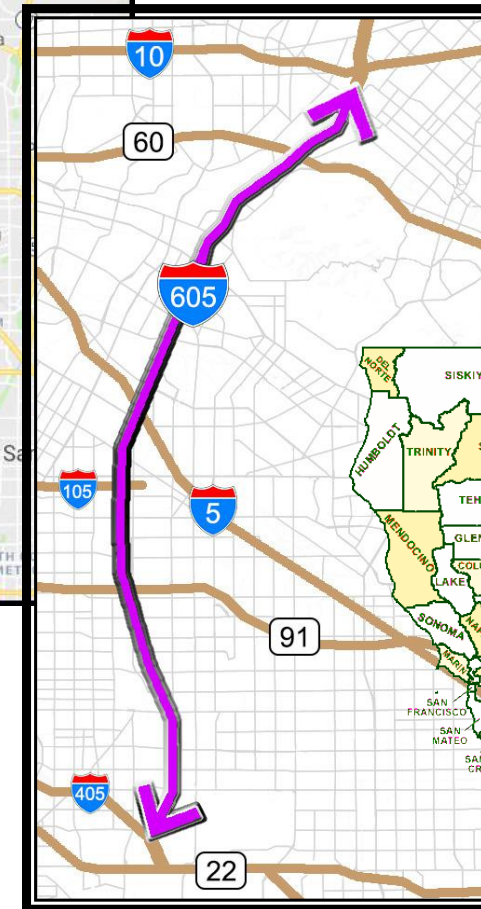
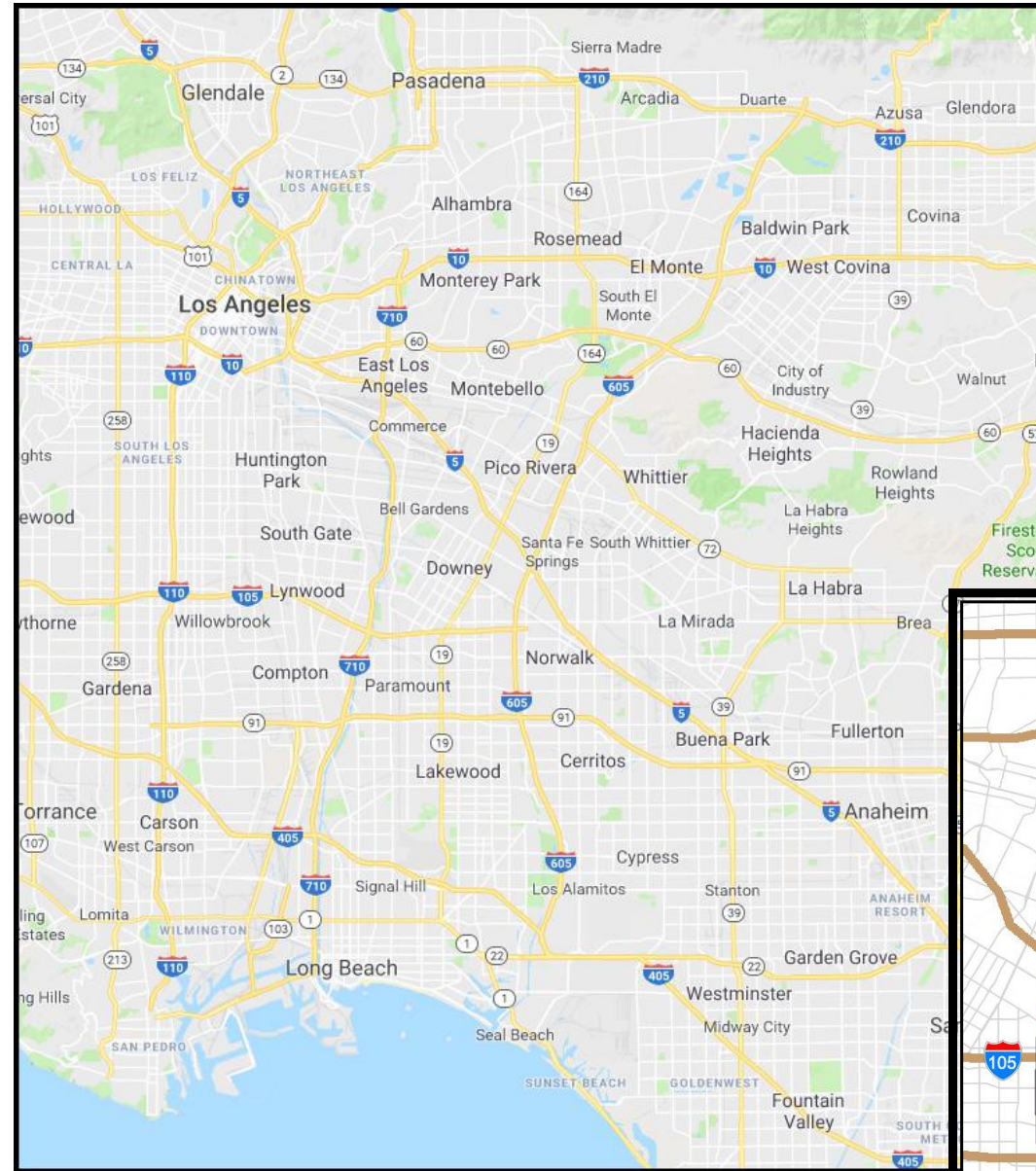
## 2.0 SITE ANALYSIS

### 2.1 Project Site

The San Gabriel Valley, approximately 280 square miles in size, is one of the principal valleys of Southern California. It is located primarily to the east of the city of Los Angeles, with a population of approximately 1.5 million people and includes 47 neighborhoods.

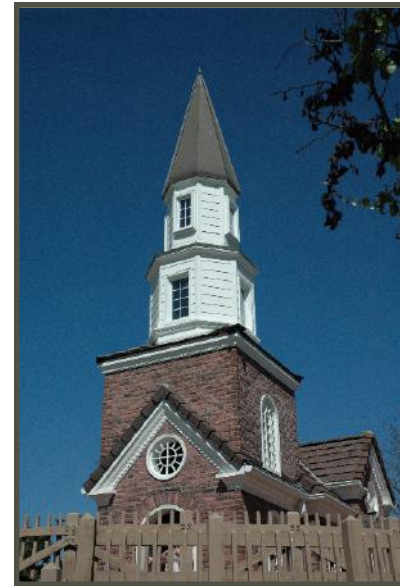
The I-605 Corridor is a pillar of the Southern California freeway system, connecting commuters, travelers, and truckers moving between the densely populated Gateway Cities, San Gabriel Valley, the Inland Empire, and the rest of the region. The I-605, known as the “San Gabriel River Freeway”, is heavily used on a daily basis for local, regional, interregional, and interstate travel and commerce, including thousands of trucks traveling between the Gateway Cities and various points inland for the distribution of goods. The Corridor is home to ethnically diverse and historically rich neighborhoods and supports thriving commercial and industrial centers. *(Courtesy of LA Metro)*

The San Gabriel Valley is home to leading educational and research institutions, such as; the California Institute of Technology (CalTech),- California State Polytechnic University, Pomona (Cal Poly), NASA’s Jet Propulsion Laboratory, and the City of Hope Medical Center. The San Gabriel Valley is a well-connected hub with seven freeways, two major rail lines, and close proximity to the Port of Long Beach and the Port of Los Angeles.



## 2.0 SITE ANALYSIS

### 2.2 Community Photos



## 2.0 SITE ANALYSIS

### 2.3 Existing I-605 Corridor





### 3.0 PROJECT AESTHETICS

Aesthetics - The science or study of the qualities of beauty, and a conception of what is artistically pleasing to the senses which may include: light, shadow, line, and color, but not limited to physical forms.

#### 3.1 Goals and Objectives

- **Create an Aesthetically Pleasing Freeway Corridor.**

The I-605 freeway is a major transportation corridor. The freeway is seen and heard by many more local residents as well. It is important to create an aesthetically pleasing corridor for all freeway users and viewers.

- **Ensure Visual Unity Within the Corridor for Motorists.**

All elements viewed predominantly by freeway users should exhibit a single, unified theme and design expression. For example, the overcrossing structures viewed mainly by freeway users should follow the same corridor design theme.

- **Enhance Community Identity.**

In areas more prominent to local travelers, such as under crossings and the community side of sound walls, aesthetic designs responding to local community needs will be allowed. In addition, expressions of community identity may be allowed on sound wall pilasters, abutment walls and other locations compatible with the goals of visual unity and safety.

- **Promote Design for Safety, Functionality, and Maintainability.**

Design for safety is a concept emphasizing the use of durable materials sited for safe access by maintenance personnel. The concept recognizes that all materials must be maintained for proper function and longevity, but smart choices will require less repetitive maintenance over the years. Proper placement of materials will allow personnel to do their work in greater safety.

- **Maintain Budget and Schedule.**

The intent of these guidelines is to give practical direction to future designers. Solutions should not require excessive capital or operational/maintenance outlay nor delay the approved schedule.

#### 3.2 Community Identification and Gateway Monuments

Community Identification is a concept developed by Caltrans to allow surrounding cities to display and express their unique attributes within the right-of-way. This can include visual images, graphics, text, or sculptural artwork. Community Identifiers are placed on structures such as: bridges, bridge abutments, bridge rails, slope paving, or retaining walls. Gateway monuments are freestanding structures or signs that communicate the name of a local city introducing the traveler to the community.



GATEWAY EXAMPLE

#### 3.3 Transportation Art

Transportation Art is a permit program for pre-existing walls such as sound walls. Art applied to walls as part of the widening project is considered an integral part of the freeway design package. Art added at a later time is subject to the Transportation Art Guidelines and is reviewed through the permit process.



#### 3.4 Funding

Many of the aesthetic treatments recommended in this report are above and beyond Caltrans standards.

As with many highway improvement projects, there will be multiple funding sources necessary to support construction of these treatments. Partial funding for the I-605 Corridor Improvement Project is through Measure R, and Measure M. LA Metro will pursue additional federal, state and other sources for future phases of this project. Additional or specialty treatments above and beyond the aesthetics proposed in this masterplan will be the responsibility of the local jurisdiction.

#### 3.5 Aesthetic Treatment Opportunities

There will be new bridge structures in the corridor. In addition, there are a number of new and replacement retaining wall and sound walls. The "Aesthetic Committee Master Plan Theme" will be applied at all structures such as: bridges, overcrossings, undercrossings, pedestrian overcrossings, viaducts, interchanges, connectors, retaining walls, sound walls, etc. Architectural treatments to these structures will be important opportunities to beautify and enhance visual continuity in the corridor. All new structures shall be integrally colored concrete to closely match Davis Colors color #5237 (Sandstone).

The existing landscaping will be removed by freeway widening. Replacement plantings will be implemented. However, many sections of the widened freeway will be paved from wall to wall or fence to fence because the spaces created will be too narrow to maintain safely. The new planting will be mainly in the interchange areas.

#### 3.6 Guidelines for Structures

This section contains aesthetic treatments that will be applied to major structural elements throughout the I-605 Project.

- **Bridges**

A beautiful bridge makes a minimal impression on the environment and is harmonious to its location. It possesses sound visual character and maintains good proportions both in its integral and spatial components. It is composed of one dominant structural system using a minimum number of bents with minimal columns per bent.

### 3.0 PROJECT AESTHETICS

Architectural and aesthetic design principles such as balance, scale, movement, repetition, emphasis and unity are utilized on the superstructure to create a visually attractive bridge which achieves the desired appearance.

#### - Columns

Columns should appear to support the structure as simply as possible. The proportion of elements is important. The goal is a balance between the apparent mass of the superstructure and the size of the column. Columns that are too thin will look spindly, and columns that are too thick will look too squat.

#### - Slope Paving

The selection of materials under bridges can be varied. Naturally colored river rock embedded in mortar can be designed to be compatible with the established corridor color palettes, textures, and themes. Alternative slope paving treatments for surfaces facing the community could include artistic relief sculptures, patterns, murals, etc.

#### - Retaining Walls

Retaining walls within the corridor shall have textured form-lined surfaces; utilizing both typical and customized textures and patterns. The form liner method affords flexibility for innovative design, as well as adding interest to retaining wall surfaces. The elevation of the retaining wall should be continuous and compliment the earth it is retaining. Walls that have extreme height variances should be modified to create smoother transitions. Walls that have sharp angles or “kinks” should be rounded and smoothed to create a more fluid appearance.

#### - Soundwalls

Viewed from the community side, bridge soundwalls are highly visible and imposing because of their height and alignment with undercrossing streets. These large horizontal walls are attached to the bridge barrier and will be seen by the traveling public at much lower speeds than on the freeway side.

#### - Concrete Barriers

Concrete barriers will be constructed in the median as well as both sides throughout the majority of the corridor for traveler safety. From the community side, in most of the sections, there will be only a very narrow 2-3 feet strip behind the barrier. It is recommended that the area be planted with low growing shrubs to provide relief from the hardscape. Such plantings outside of state right-of-way require a cooperative agreement stipulation that a local entity will maintain the planting after Caltrans installs it.

#### - Lighting

When selecting lighting, maintenance and vandalism should be considered. Two types of lighting may be used at overcrossings, either the Caltrans standard, or upgraded decorative lighting. All undercrossings should have recessed lighting above pedestrian walkways for safety and security. Lighting beyond Caltrans standards on overcrossings or undercrossings shall be relinquished to the cities. Therefore, the choice of lighting on bridges throughout the corridor will be dependent on the city with jurisdiction over that area.

#### - Fencing

Three types of fences are proposed for use in the I-605 corridor: I-605 theme fencing, Caltrans standard chain link, and post and cable. The I-605 theme fencing consists of an arch design, 1-inch grid of vinyl-coated mesh, black painted finish, and will be used at structures and overcrossings. It is also recommended in areas with high visibility to freeway users including areas where the views toward businesses on the frontage road have been preserved.

The theme fence will be located on top of the concrete barrier, and top of curb (as with pedestrian overcrossings). Galvanized chain link fence per Caltrans standard specifications will be used to divide the corridor right-of-way from the local areas in order to prevent pedestrians from entering the freeway right-of-way. Post and cable fencing (per Caltrans standard specifications and OSHA requirements) will be used along the top of retaining walls to enhance the safety of freeway maintenance workers.

#### - Paving/Hardscape

The color, texture, pattern and material of hardscape at various locations should be coordinated. Rock blanket is the preferred gore paving material for consistency with the newly widened I-5 south of the Orange County line.

Gore areas less than 30 feet wide and narrow areas less than 20 feet wide can be difficult locations for maintenance personnel to work in. These areas are considered hard to maintain. Paving of gore and narrow areas is recommended.

#### - Utilities

Electrical boxes, monitoring equipment, pump houses and other facilities associated with utilities should be consolidated and their placement integrated with the surrounding landscape.

#### - Structural Components

Note: Important consideration must be given to the detailing of concrete surface treatment to prevent any conflicts with the component's structural section. This includes barriers, wing walls, abutments, slope paving, exterior girders, retaining walls, and columns. The structural component's design shall maintain specific clearances from the concrete's surface to the reinforcement. See “California Amendments to AASHTO LRFD Bridge Design Specifications” page 5-189A, Table 5.12.3-1 and “Bridge Design Details” Section 1-4.1. The surface treatment must not intrude into this clearance. Also, the details provided are intended as examples only. They are not to be used for specific designs.

NOTE: ALL ARCHITECTURAL DETAILS ARE APPROVED FOR AESTHETIC PURPOSES ONLY.

#### 3.7 Guidelines for Other Elements

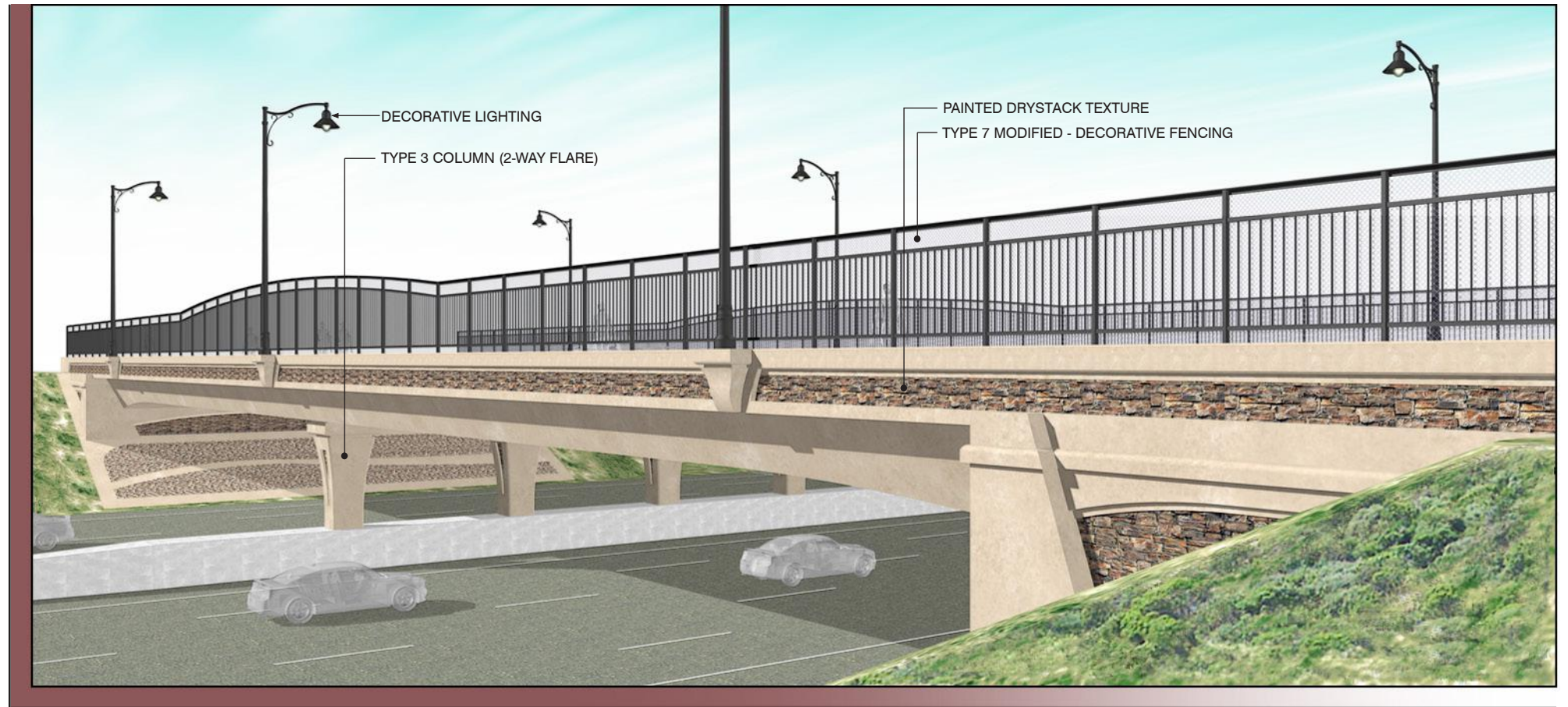
In addition to landscaping and major structural elements of bridges, sound walls and retaining walls, other elements such as undercrossings, fences, paving, lighting, utility cabinets and community art together make an impact on the overall appearance of the freeway corridor. These elements should be coordinated and integrated into the corridor theme. The treatment as well as placement of these elements should be considered and reviewed as early as possible.

## 4.0 STRUCTURAL AESTHETICS

### 4.1 OVERCROSSINGS

An overcrossing is a structure carrying a county road or a city street over a state highway, and occurs at various locations along the I-605 Corridor.

Replaced or widened bridges will often be intermixed with existing bridges which have different architectural styles. The overall geometry of replaced or widened structures will be similar to remaining structures to provide visual unity. The “Aesthetic Committee Master Plan Theme” will be applied to geometric forms of girders, columns, and abutment walls which responds to the historic and geographic influences of the region.

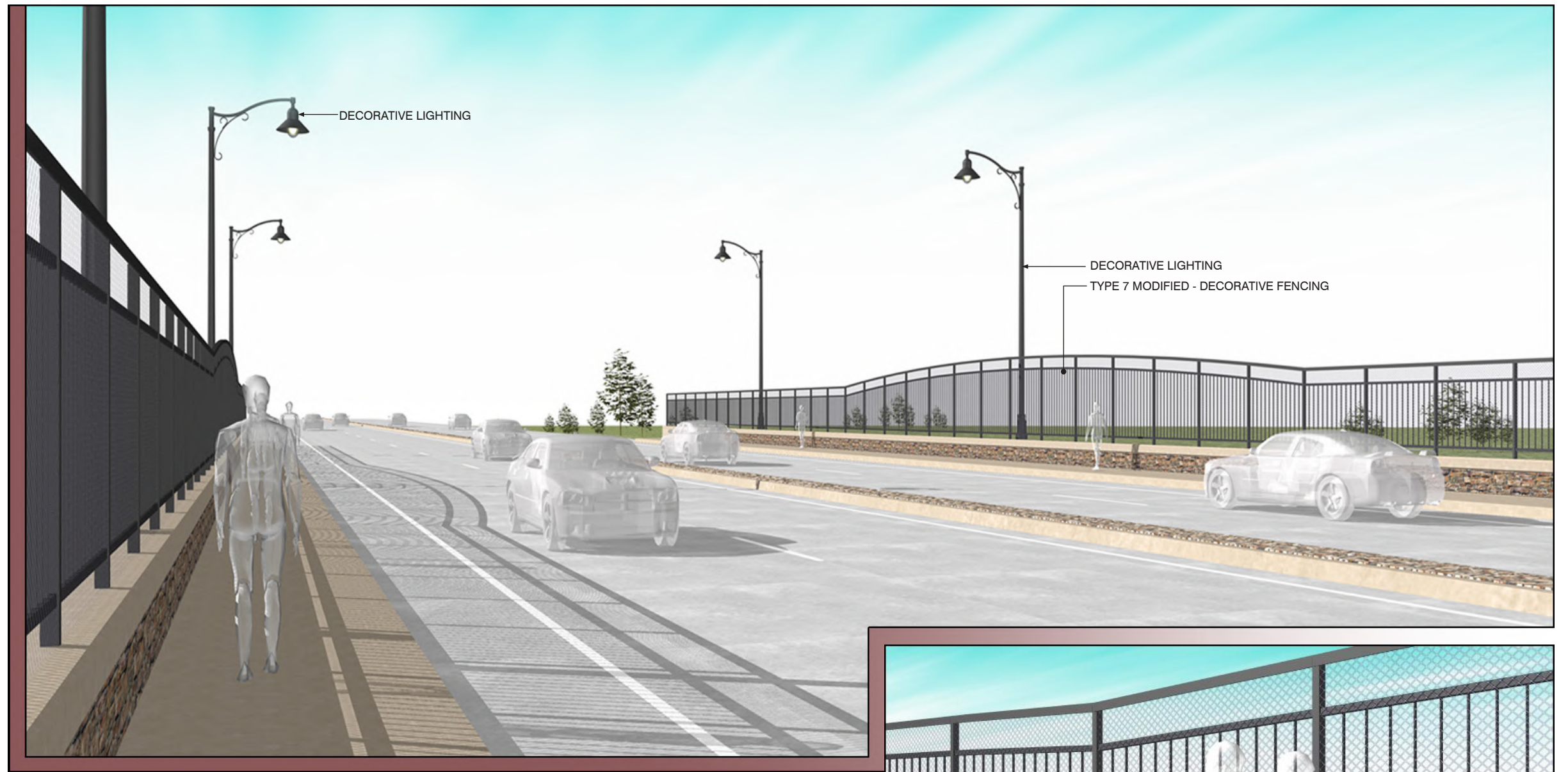
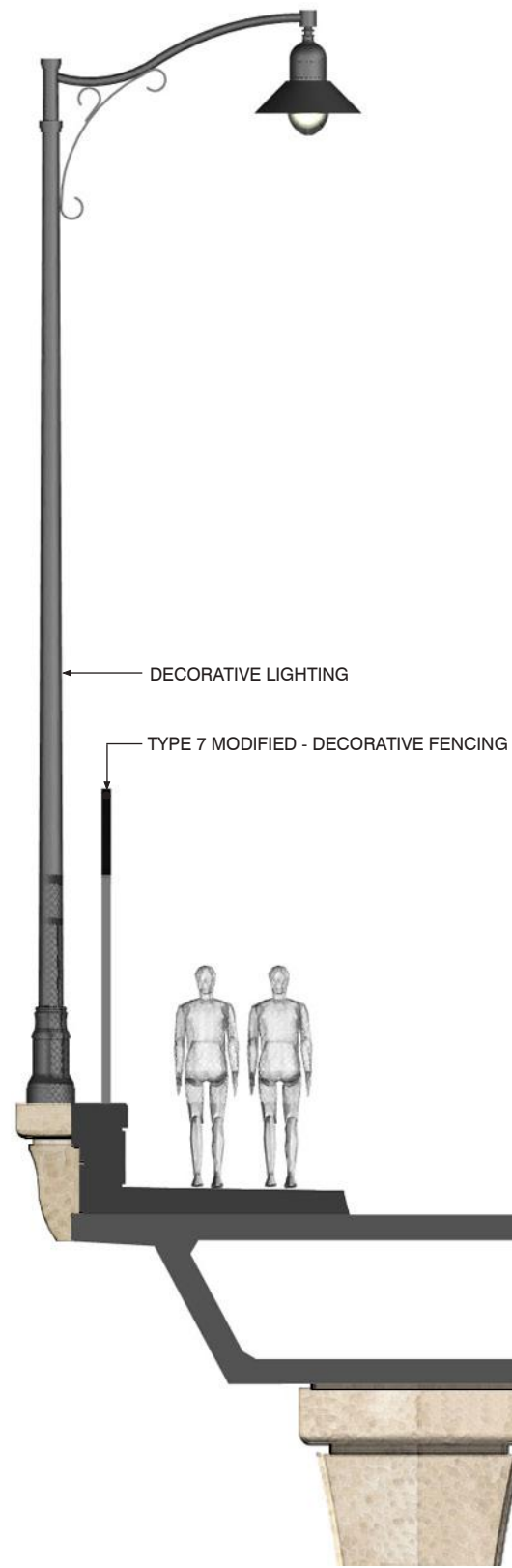


PERSPECTIVE VIEW



FREEWAY - PERSPECTIVE VIEW

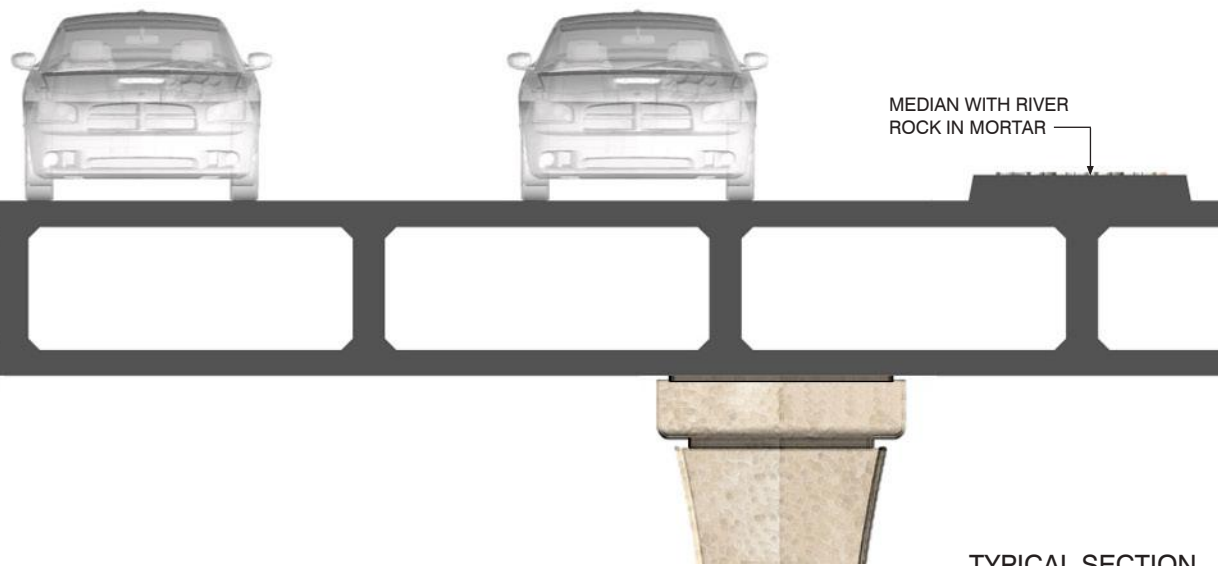
4.1 OVERCROSSINGS



PERSPECTIVE VIEW



PERSPECTIVE VIEW



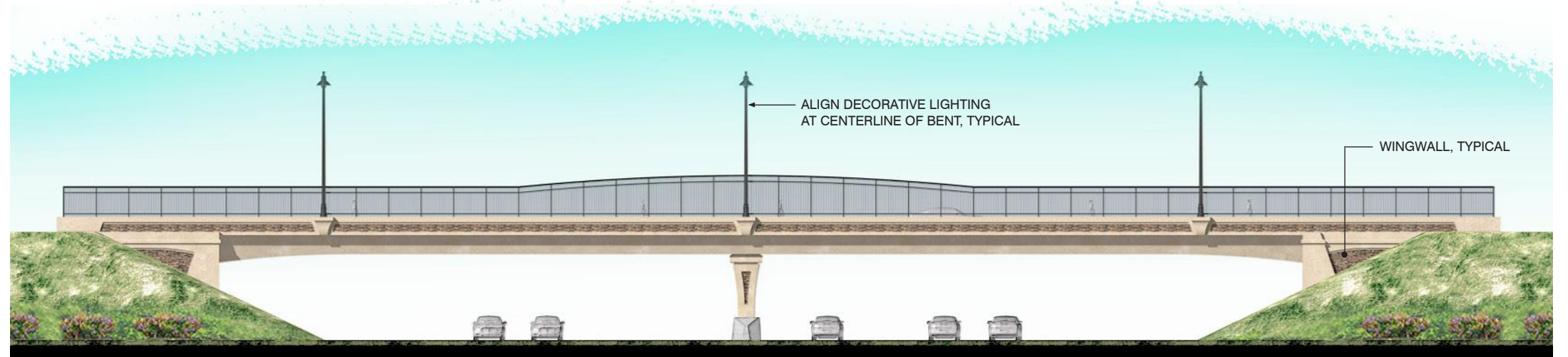
TYPICAL SECTION

FINAL

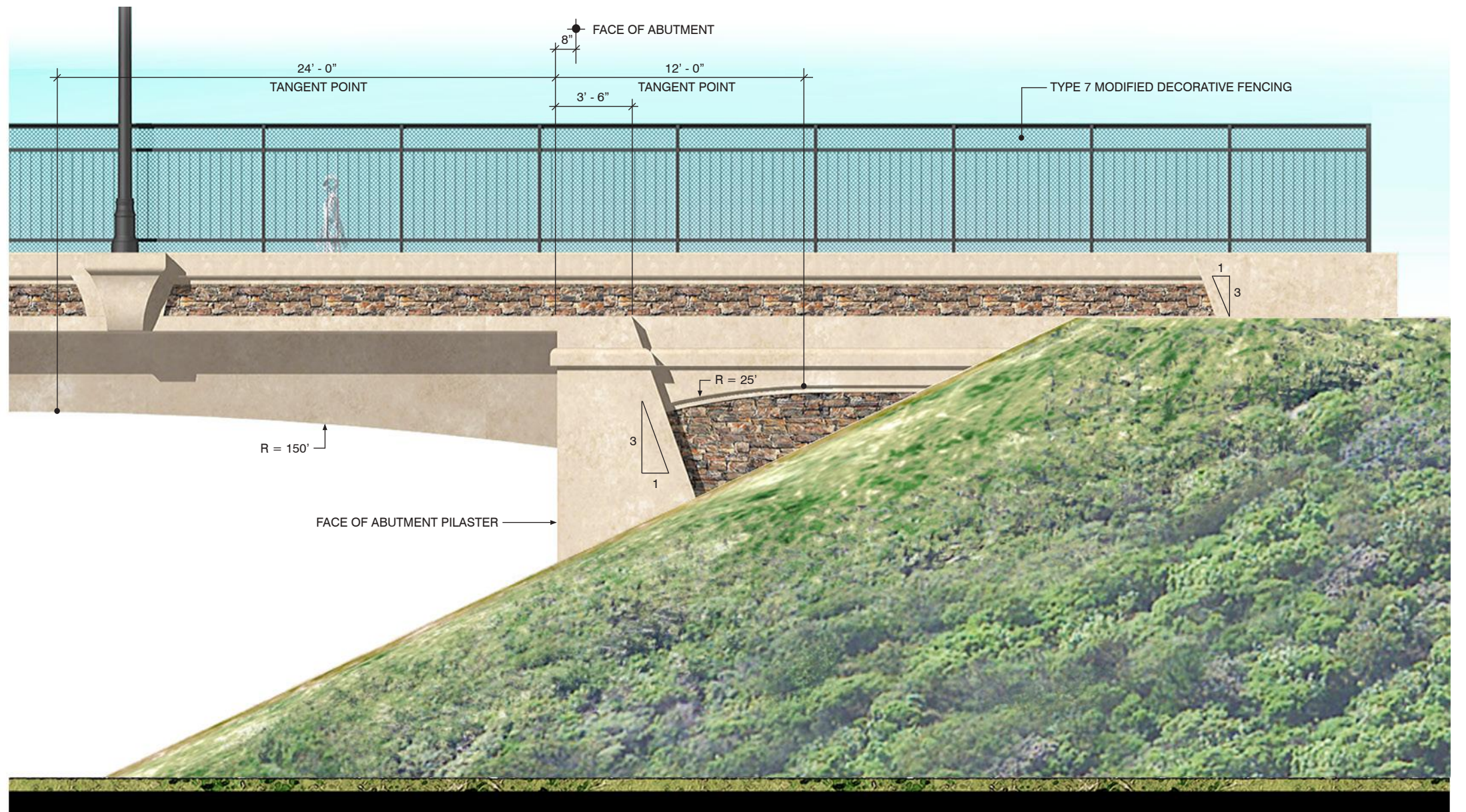
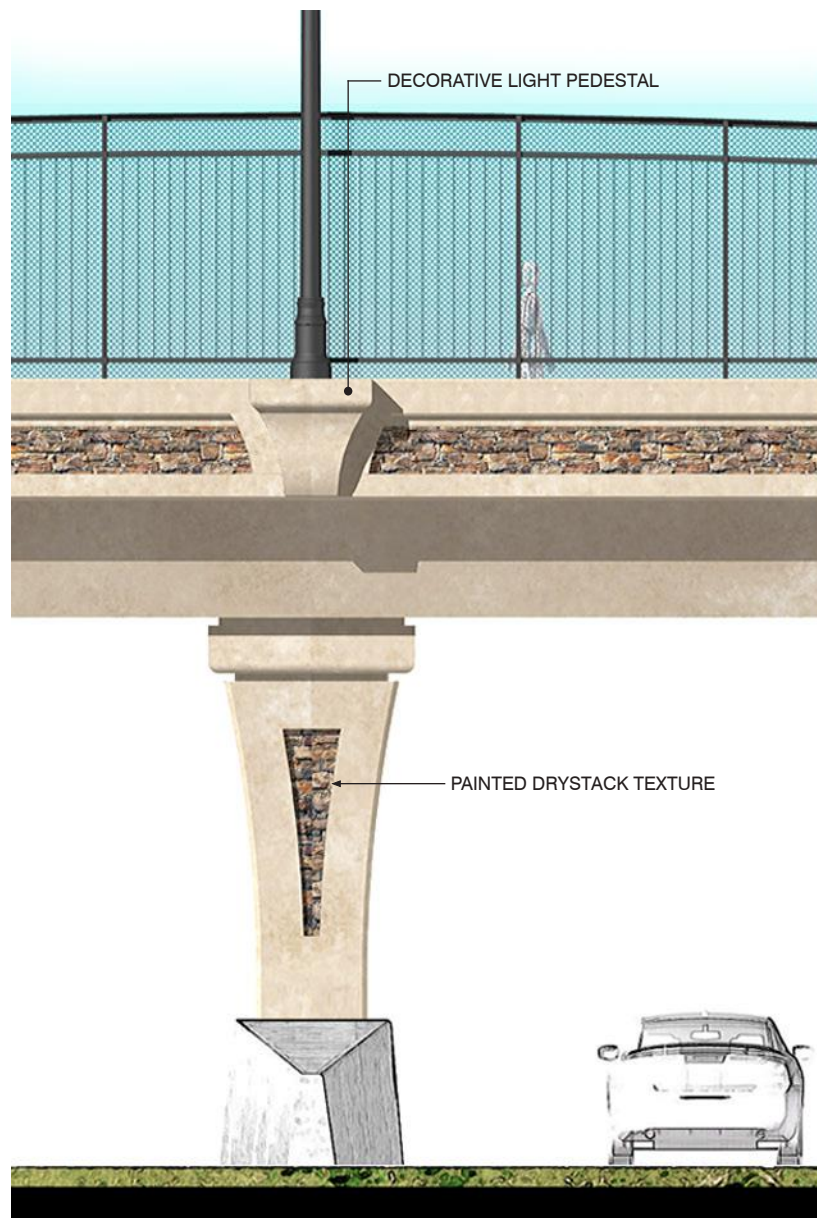
REV 2 - JUNE 13, 2019

#### 4.1 OVERCROSSINGS

Overcrossings will have haunched girders, 2-way flared columns with textured recess, lighting pedestals, and pilasters at the abutment corners. The architectural treatment consists of integrally colored concrete, painted drystack stone texture, and decorative fencing and lighting with black finish.



FREEWAY - ELEVATION VIEW



PARTIAL ELEVATION VIEW

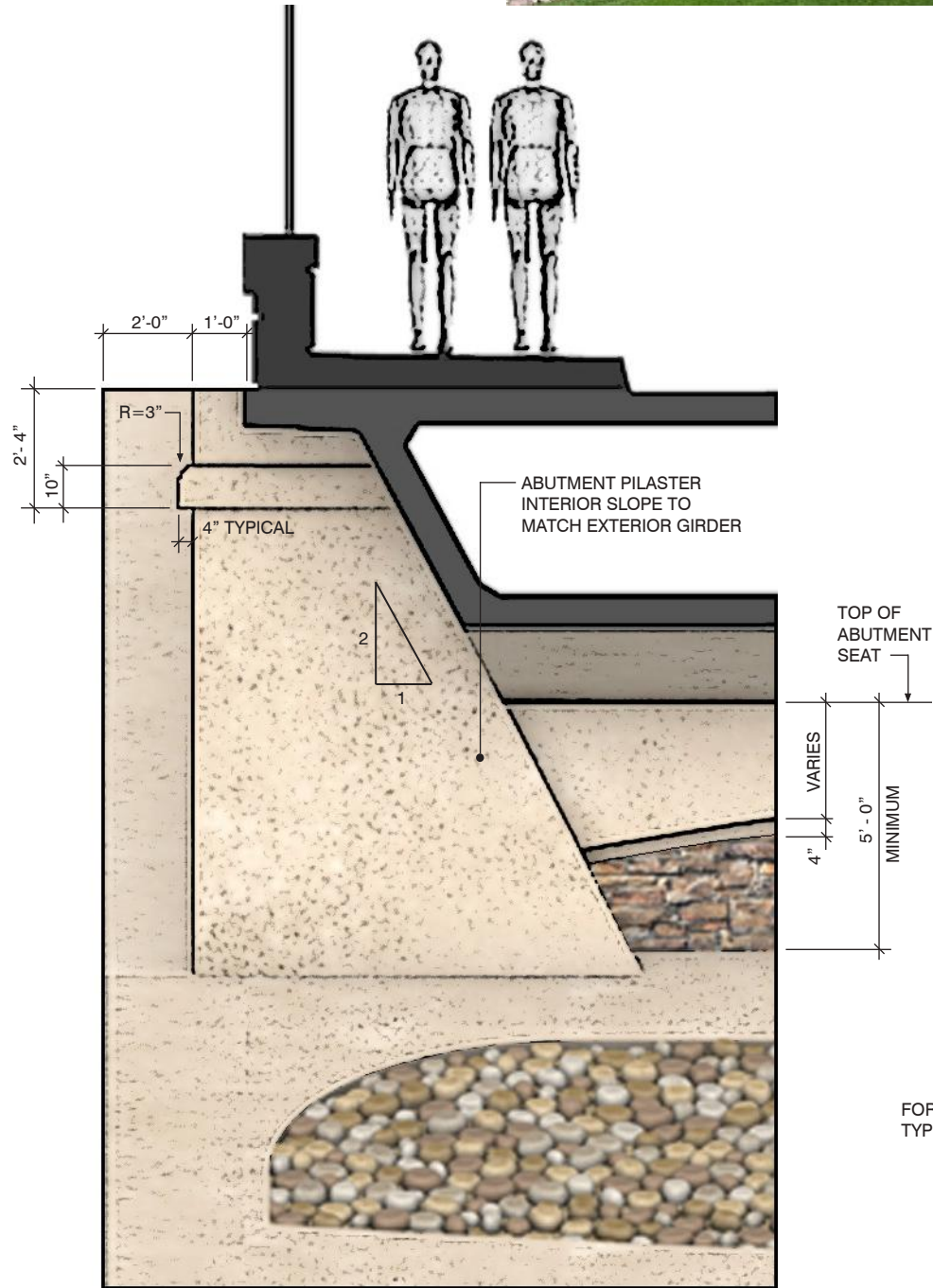
FINAL

REV 2 - JUNE 13, 2019

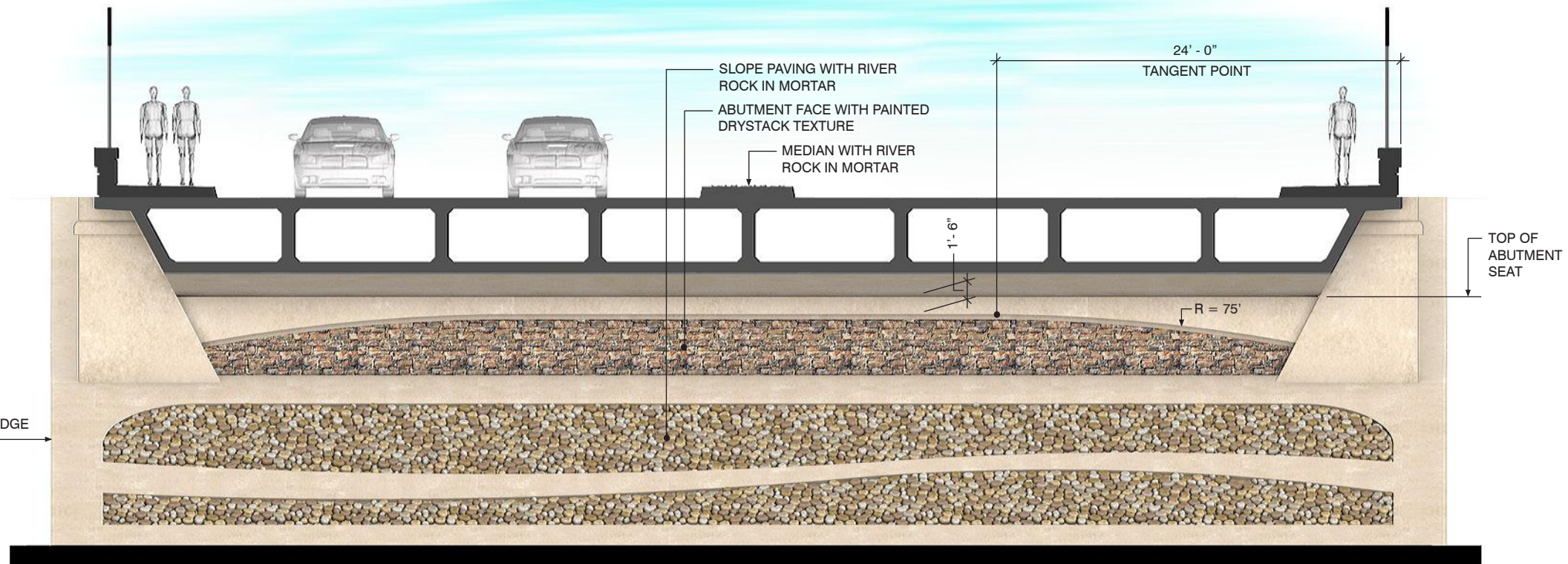
4.1 OVERCROSSINGS



PERSPECTIVE VIEW



PARTIAL SECTION



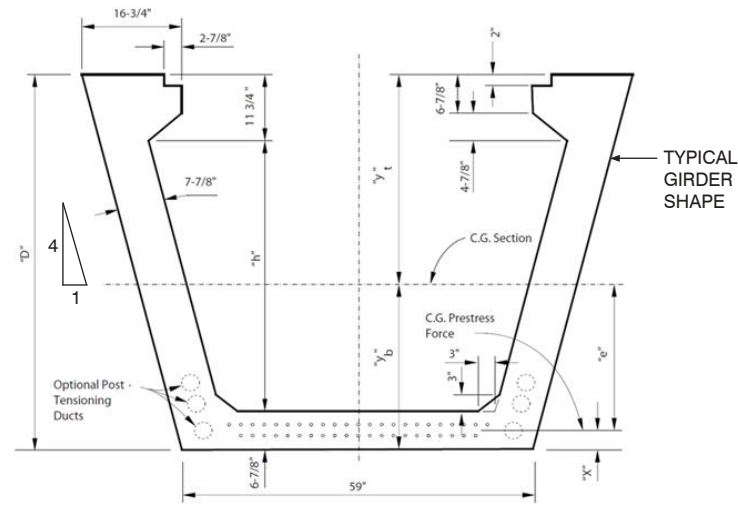
TYPICAL SECTION

FINAL

REV 2 - JUNE 13, 2019

#### 4.1 OVERCROSSINGS

When cast-in-place box girders are not possible, haunched precast bath-tub girders are preferred and should adhere to the “Aesthetic Committee Master Plan Theme”.



California Standard “Bath-Tub” Girder



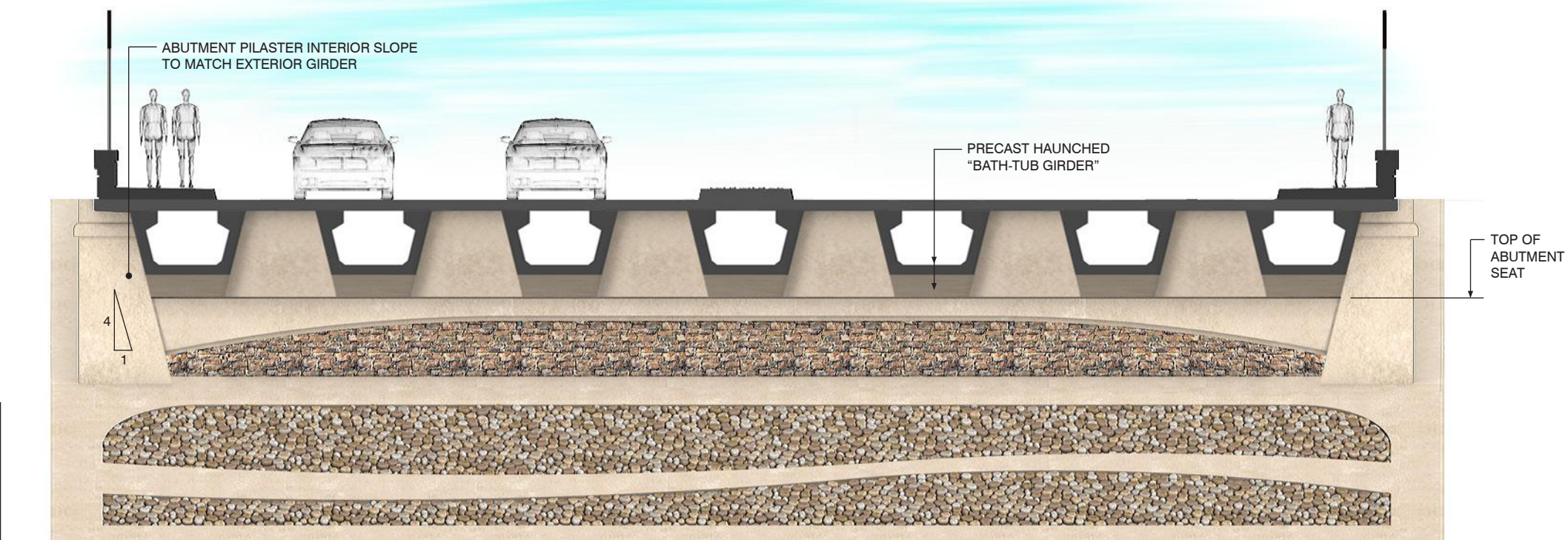
PERSPECTIVE VIEW



INTEGRAL BENT CAP

PARTIAL ELEVATION

Note: Non-integral bent caps positioned below bottom of precast girders (drop caps) are not allowed except to match existing when widening.

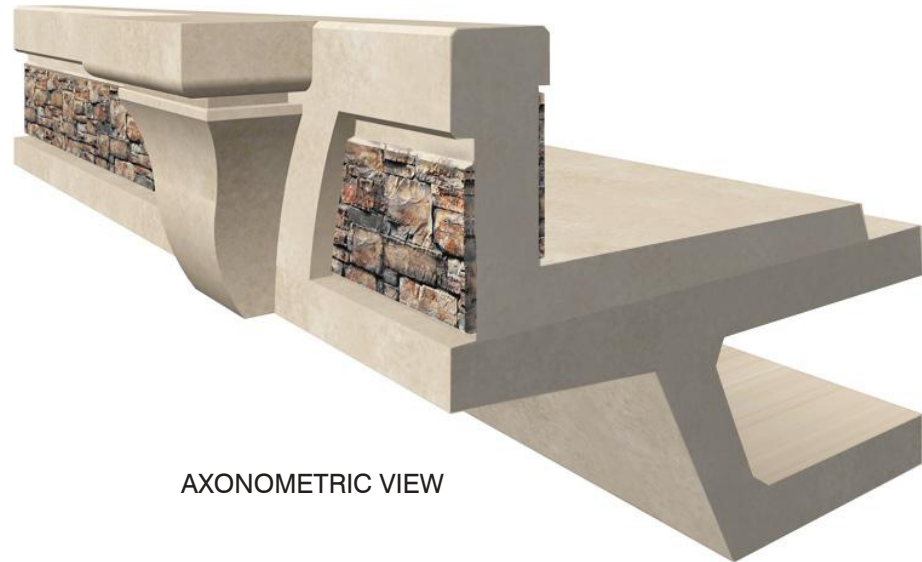


TYPICAL SECTION

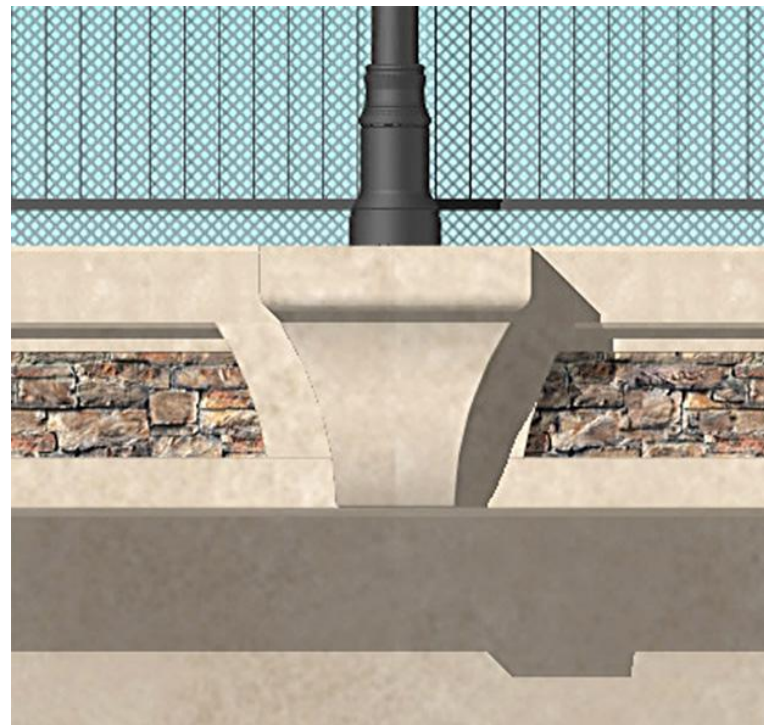
FINAL

## 4.2 BRIDGE DETAILS

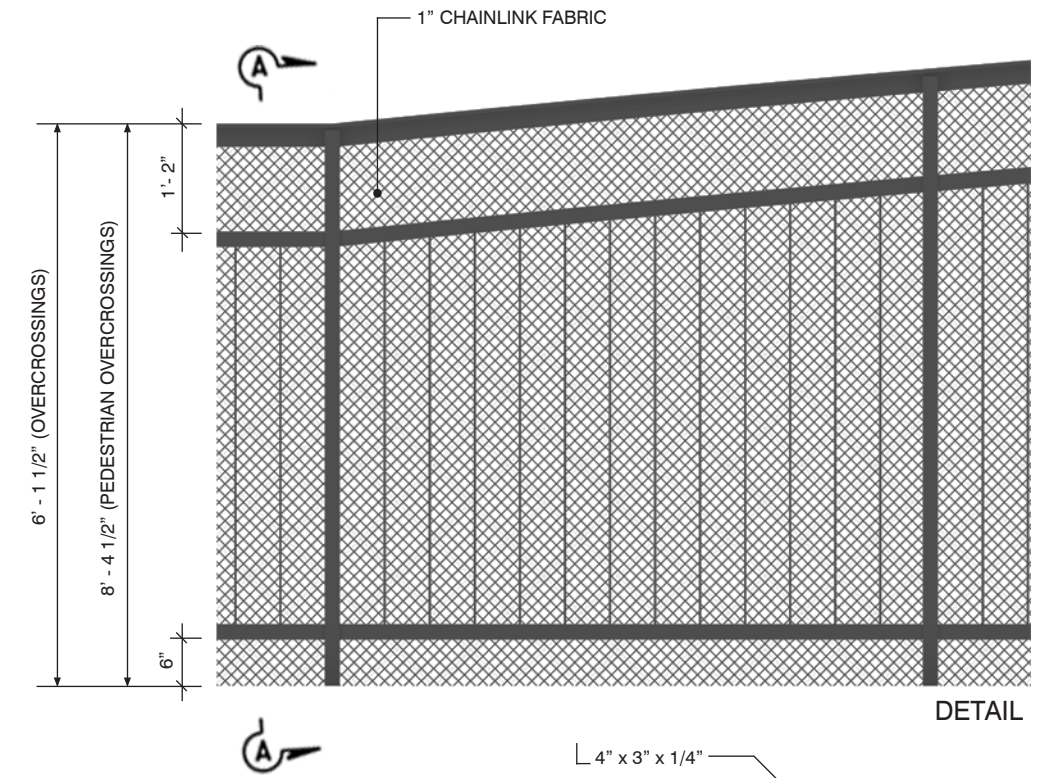
Most bridge replacements will have concrete box girders. The visible face of the girder will be sloped with a deck overhang to provide a shadowed area.



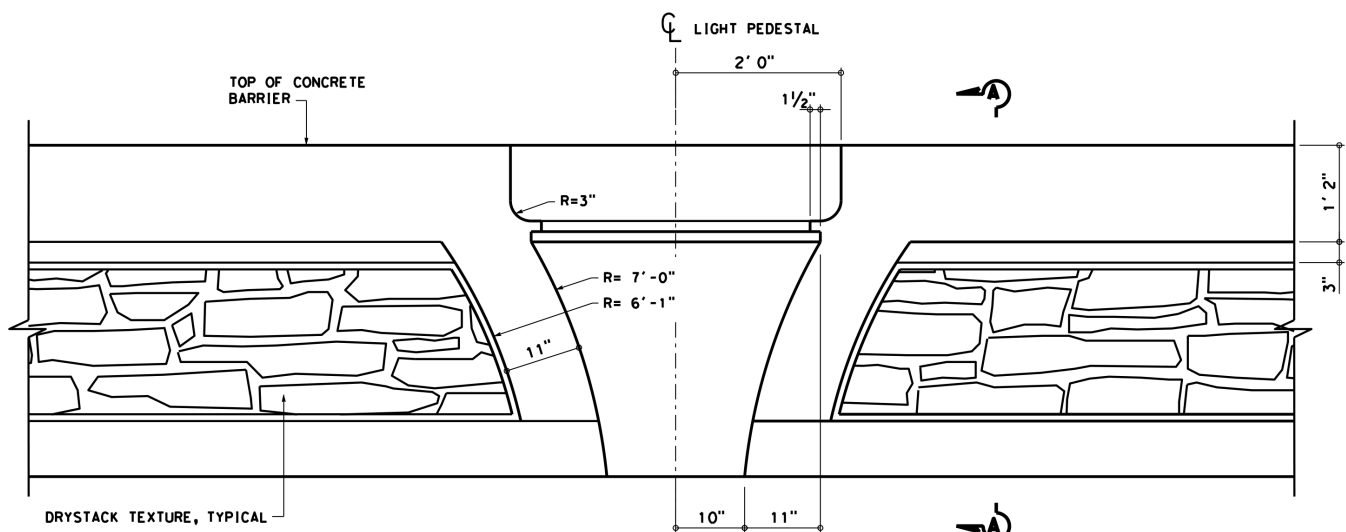
AXONOMETRIC VIEW



PARTIAL ELEVATION

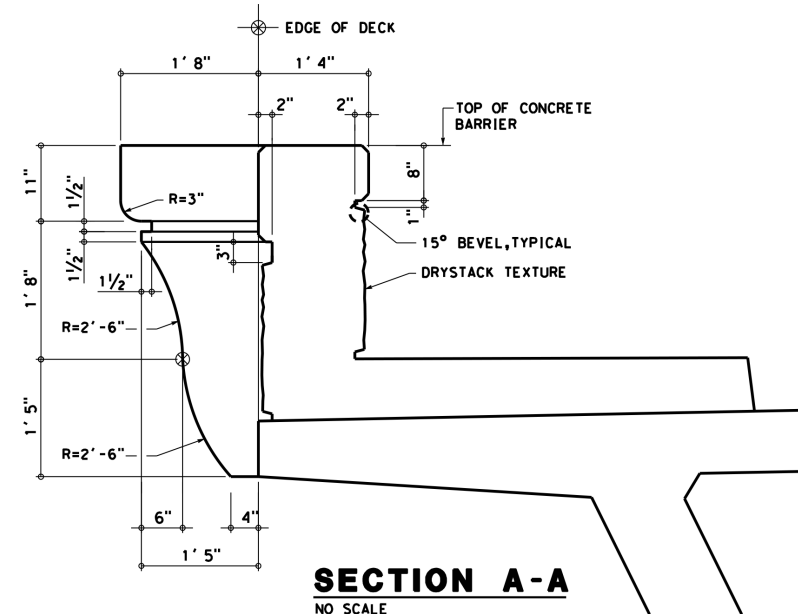


DETAIL



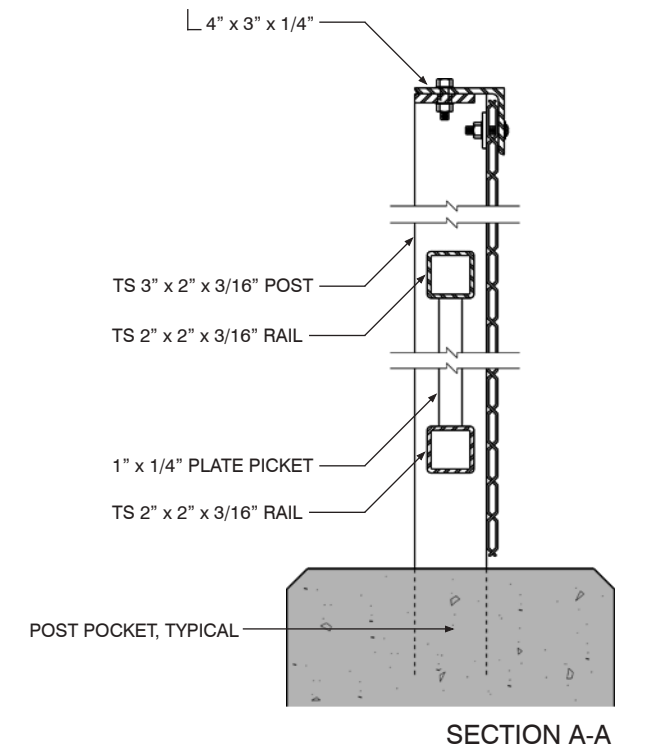
**PARTIAL ELEVATION - BARRIER MODIFIED WITH LIGHT PEDESTAL**

NO SCALE

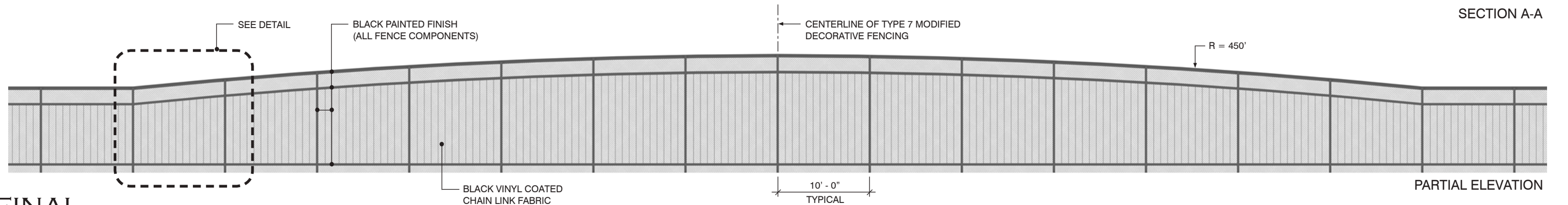


**SECTION A-A**

NO SCALE



SECTION A-A

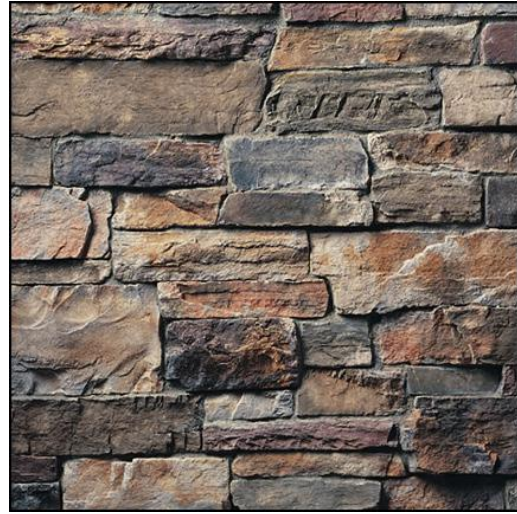


PARTIAL ELEVATION

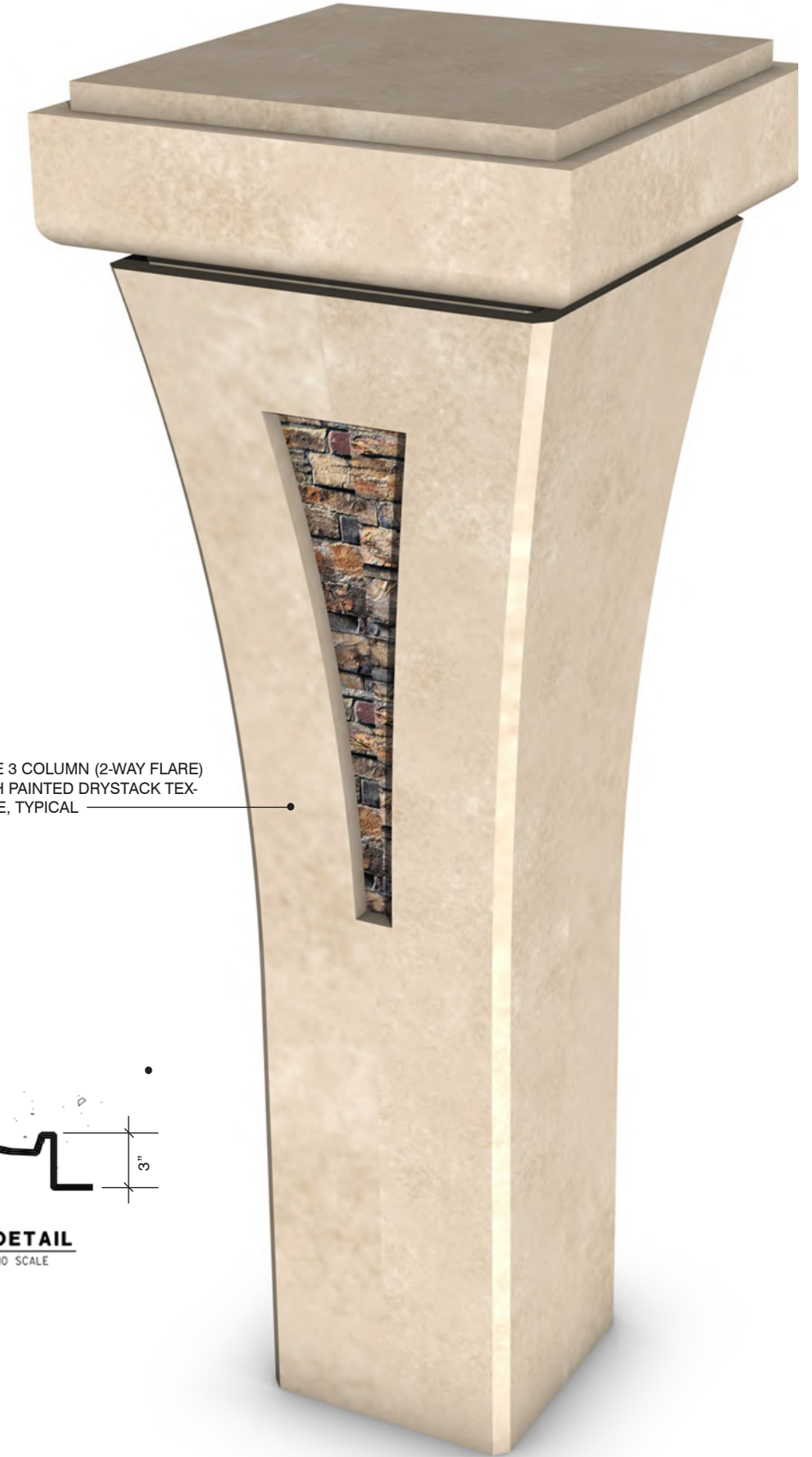
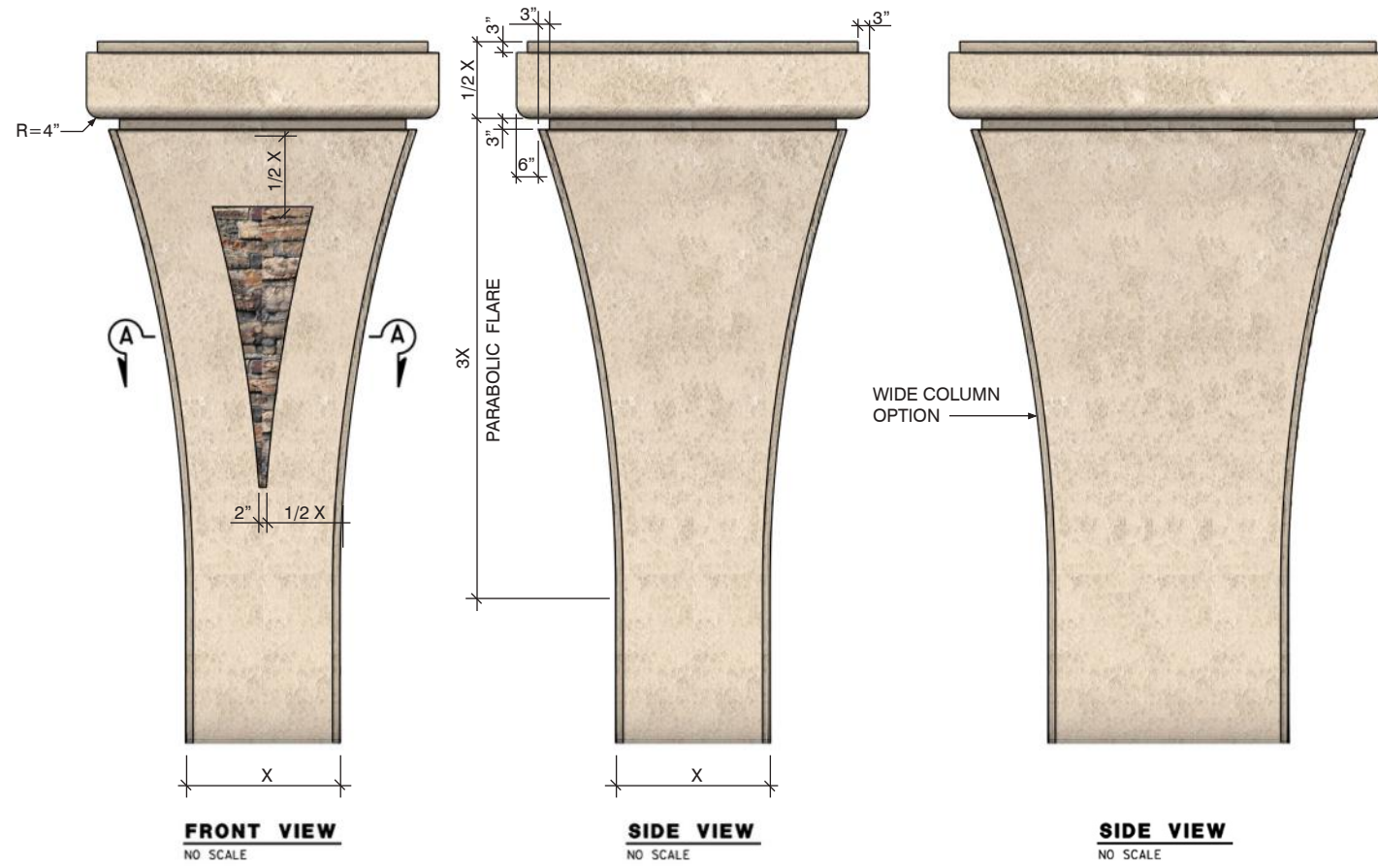
FINAL



4.2 BRIDGE DETAILS



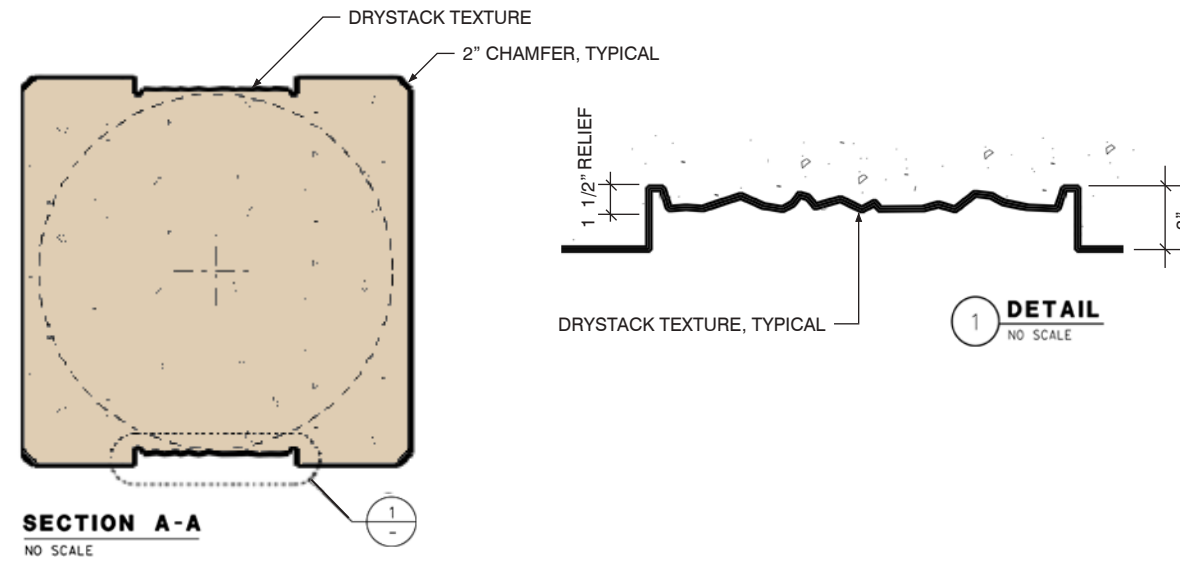
**DRystack TEXTURE**  
 TO MATCH FITZGERALD PATTERN  
 #17911 - 1 1/2" RELIEF  
 SAN DIEGO DRystack TEXTURE  
 PAINT COLORS TO MATCH #'s  
 30117, 30215, 36251, AND 37722  
 OF FED-STD-595



TYPE 3 COLUMN (2-WAY FLARE)  
 WITH PAINTED DRystack TEX-  
 TURE, TYPICAL



AESTHETIC MOTIF INFLUENCES



AXONOMETRIC VIEW

FINAL

4.3 UNDERCROSSINGS

The “Aesthetics Committee Master Plan Theme” will be applied to all undercrossings. The architectural treatment consists of integrally colored concrete, painted drystack stone texture, basalt texture, colored horizontal band representing the river, and project fencing and lighting with black finish.



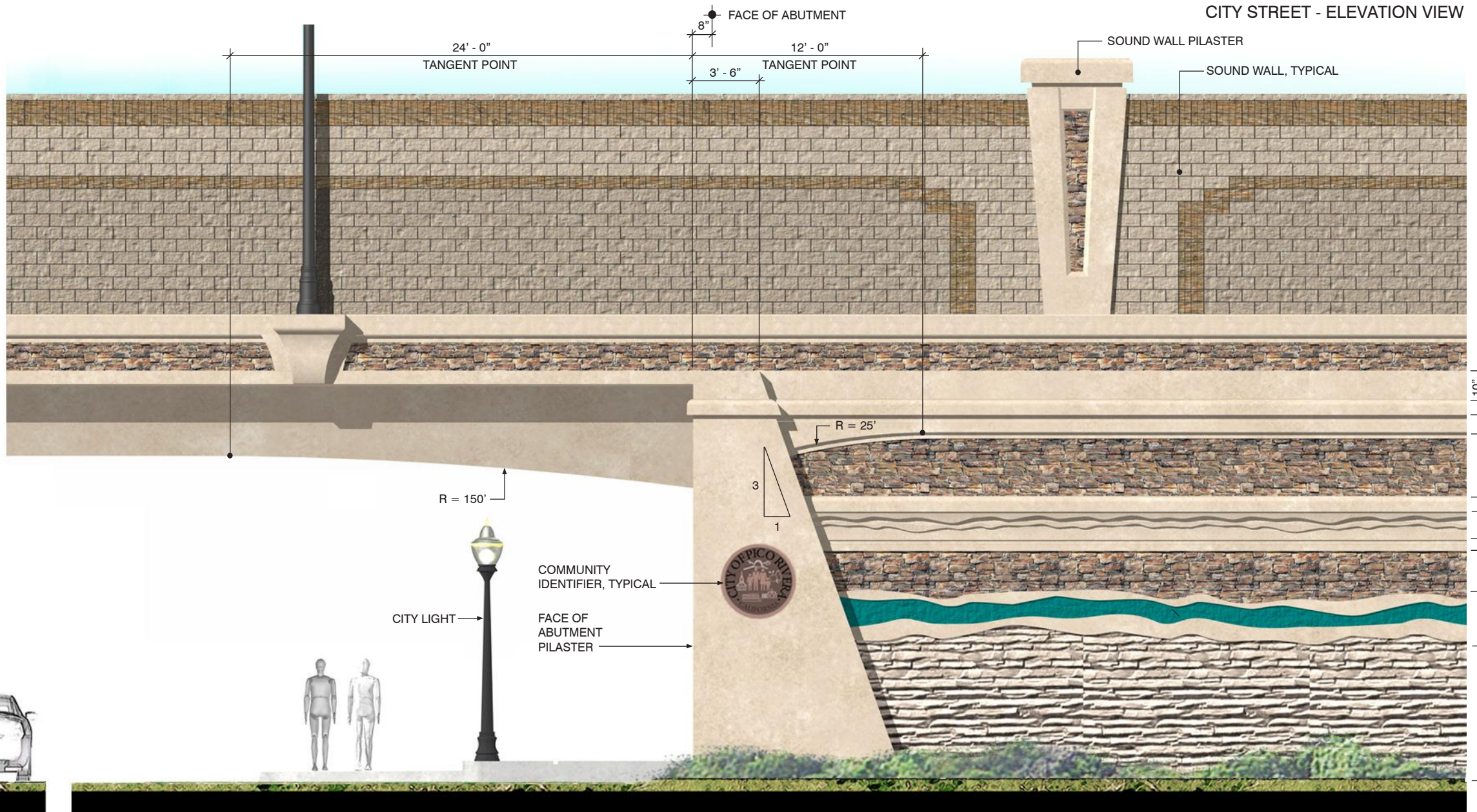
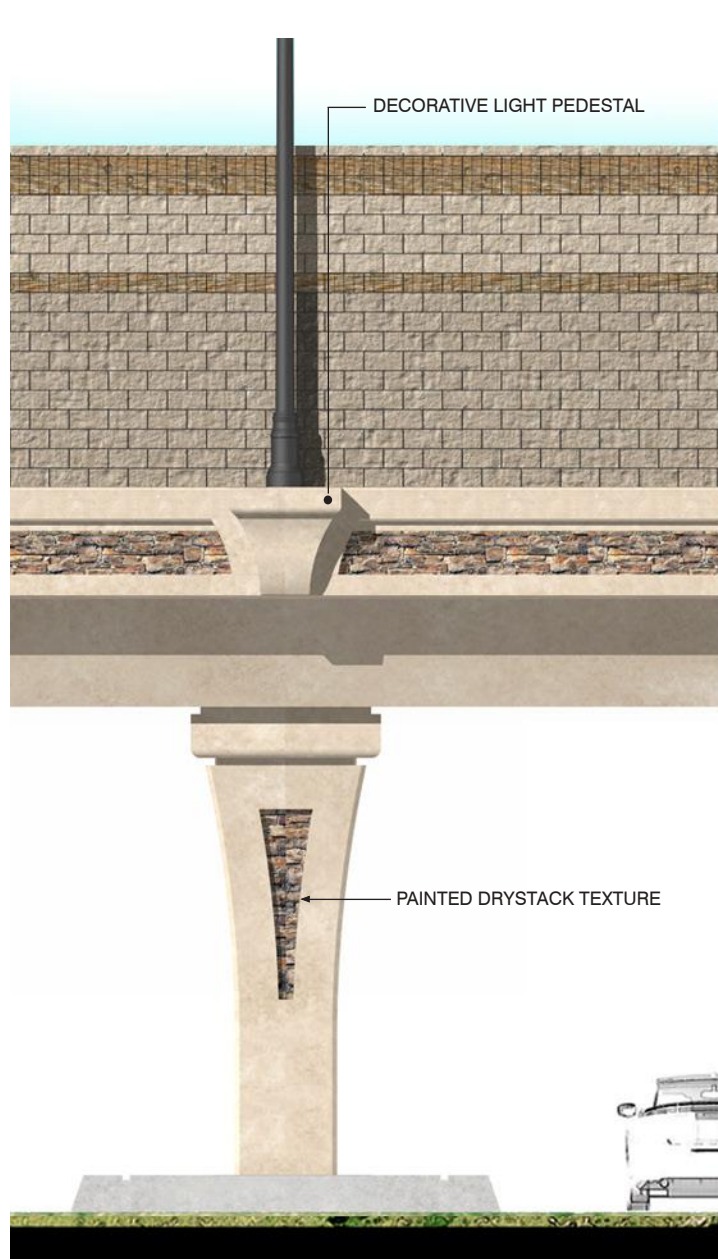
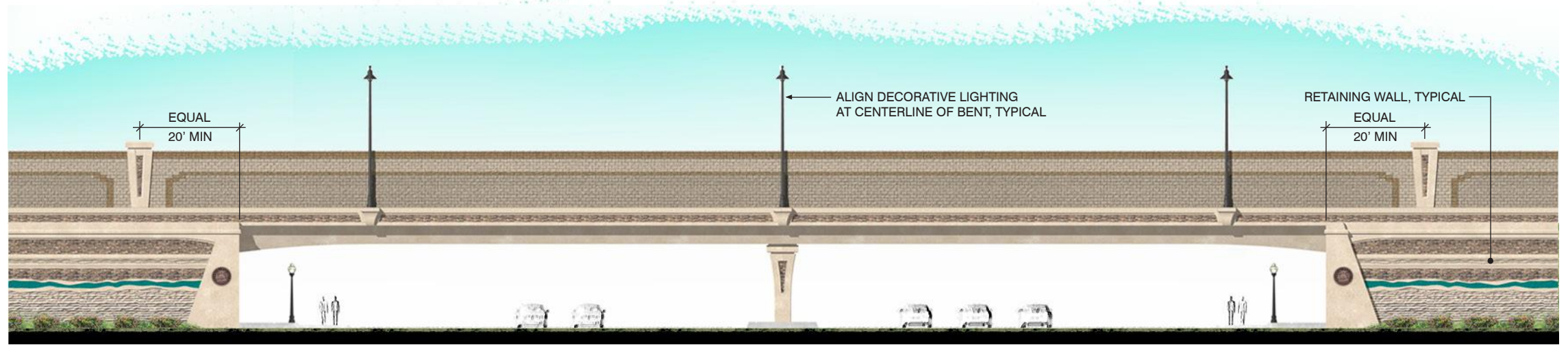
PERSPECTIVE VIEW



CITY STREET - PERSPECTIVE VIEW

FINAL

4.3 UNDERCROSSINGS



CITY STREET - ELEVATION VIEW

PARTIAL ELEVATION VIEW

FINAL

4.3 UNDERCROSSINGS

Bridge corner pilasters will be used to frame bridges and provide opportunity to incorporate community graphic castings. Each city along the I-605 Corridor Project will have an opportunity to incorporate their city seal into the pilaster to create a community identifier welcoming travelers and visitors.

Note: This feature to be negotiated by LA Metro, Caltrans, and the individual cities.



CITY SEAL EXAMPLE



PERSPECTIVE VIEW



PERSPECTIVE VIEW



TYPICAL SECTION

FINAL

REV 2 - JUNE 13, 2019

4.3 UNDERCROSSINGS



BIRD'S EYE VIEW

FINAL

REV 2 - JUNE 13, 2019

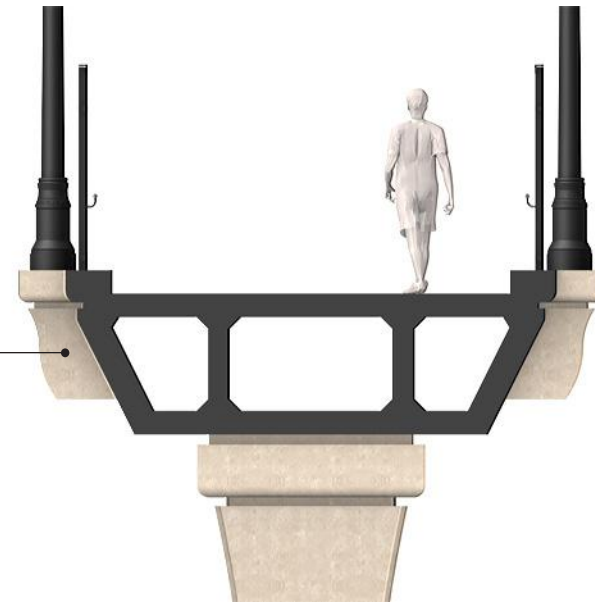
#### 4.4 PEDESTRIAN OVERCROSSINGS

The “Aesthetics Committee Master Plan Theme” will be applied to all pedestrian overcrossings. The architectural treatment consists of integrally colored concrete, painted drystack stone texture, and project fencing and lighting with black finish.



PERSPECTIVE VIEW

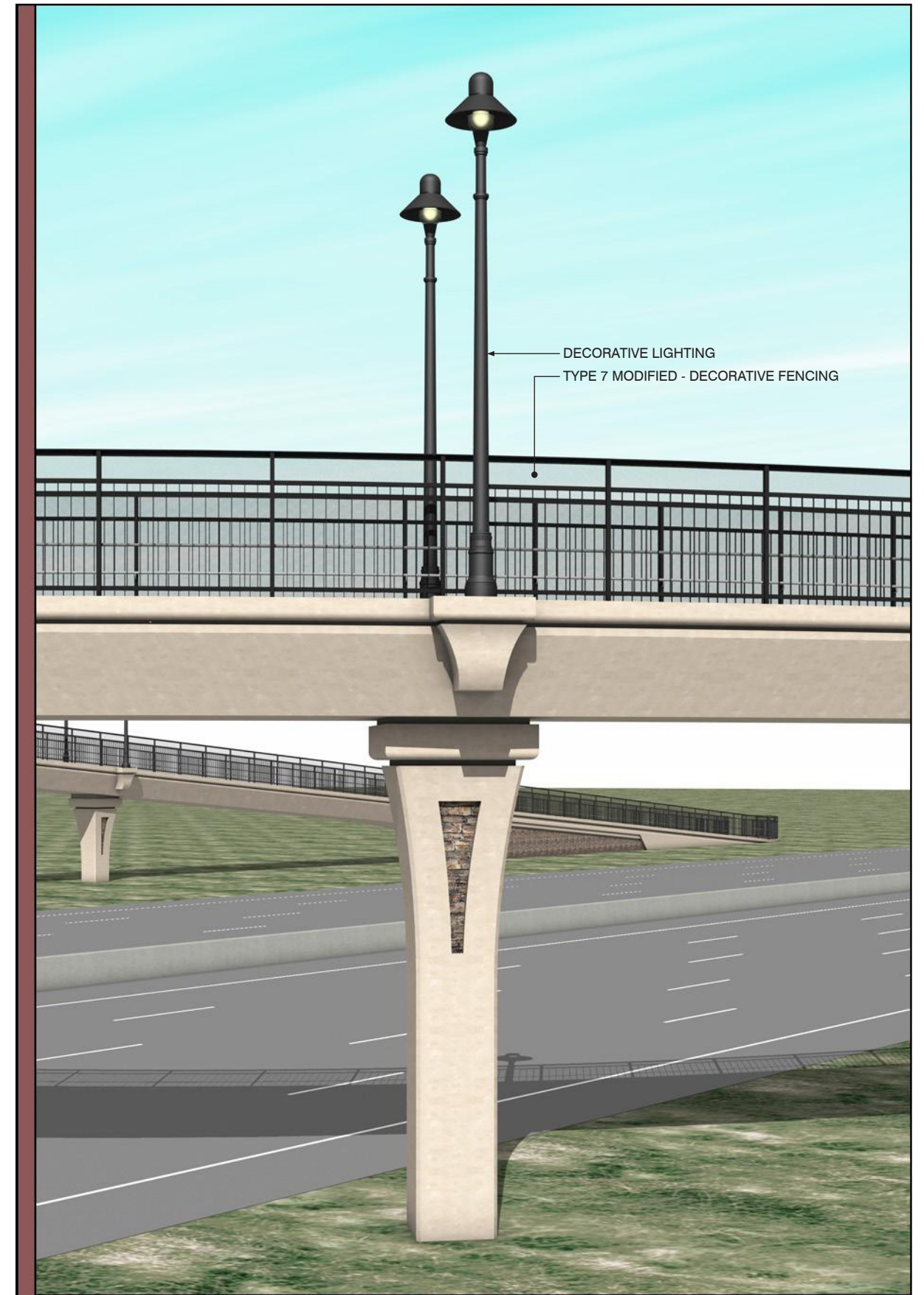
DECORATIVE LIGHT PEDESTAL



TYPICAL SECTION



PERSPECTIVE VIEW



DECORATIVE LIGHTING  
TYPE 7 MODIFIED - DECORATIVE FENCING

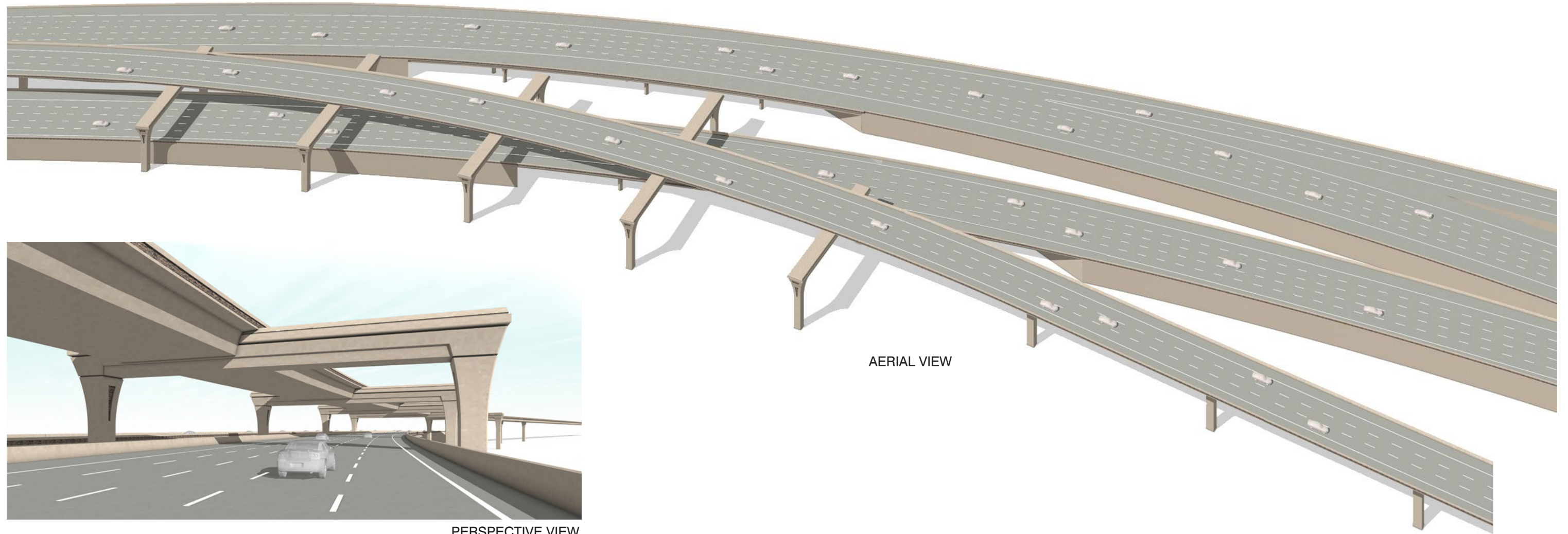
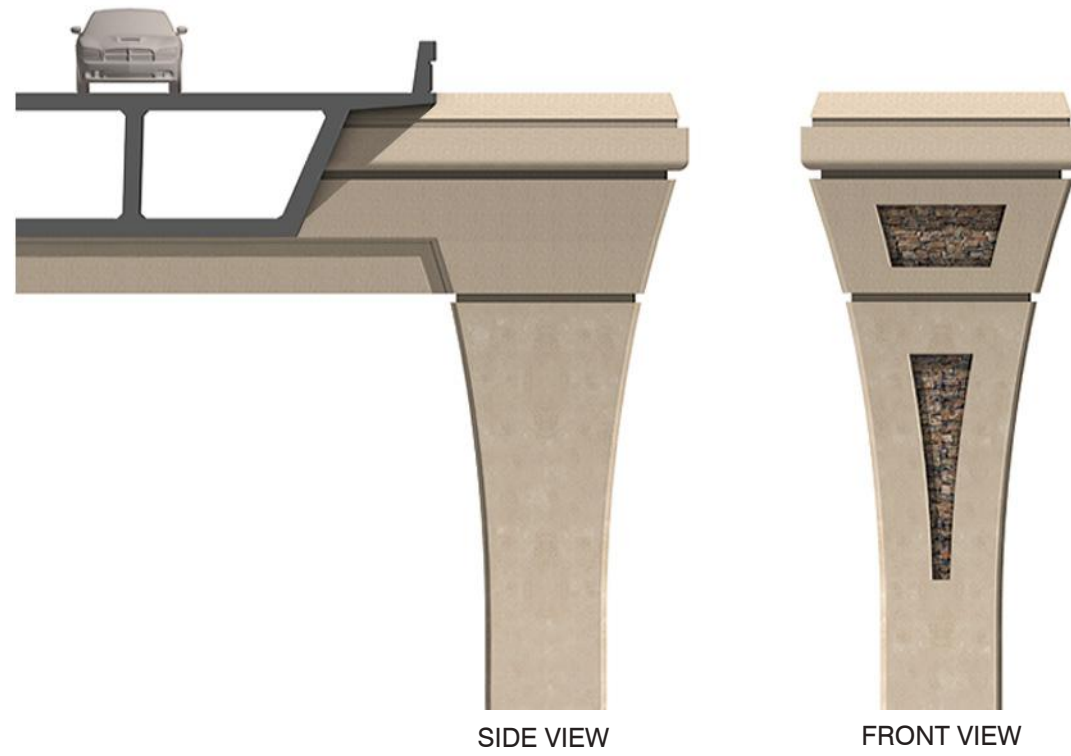
PERSPECTIVE VIEW

FINAL

REV 2 - JUNE 13, 2019

#### 4.5 CONNECTORS

Several interstates and freeways cross paths with the I-605 Corridor. At these intersecting locations, consideration must be given to the convergence of different route design themes. The I-605 Aesthetics Committee will review and approve all designs for interchanges and connectors.



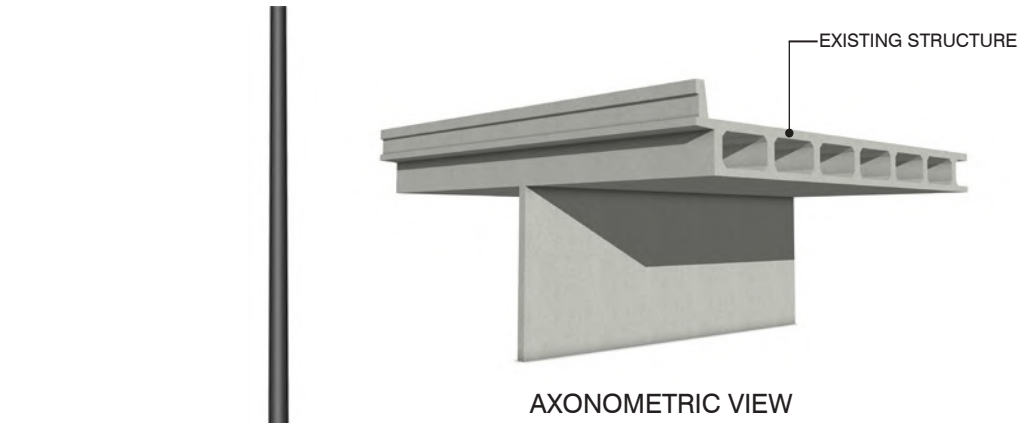
FINAL

#### 4.6 BRIDGES

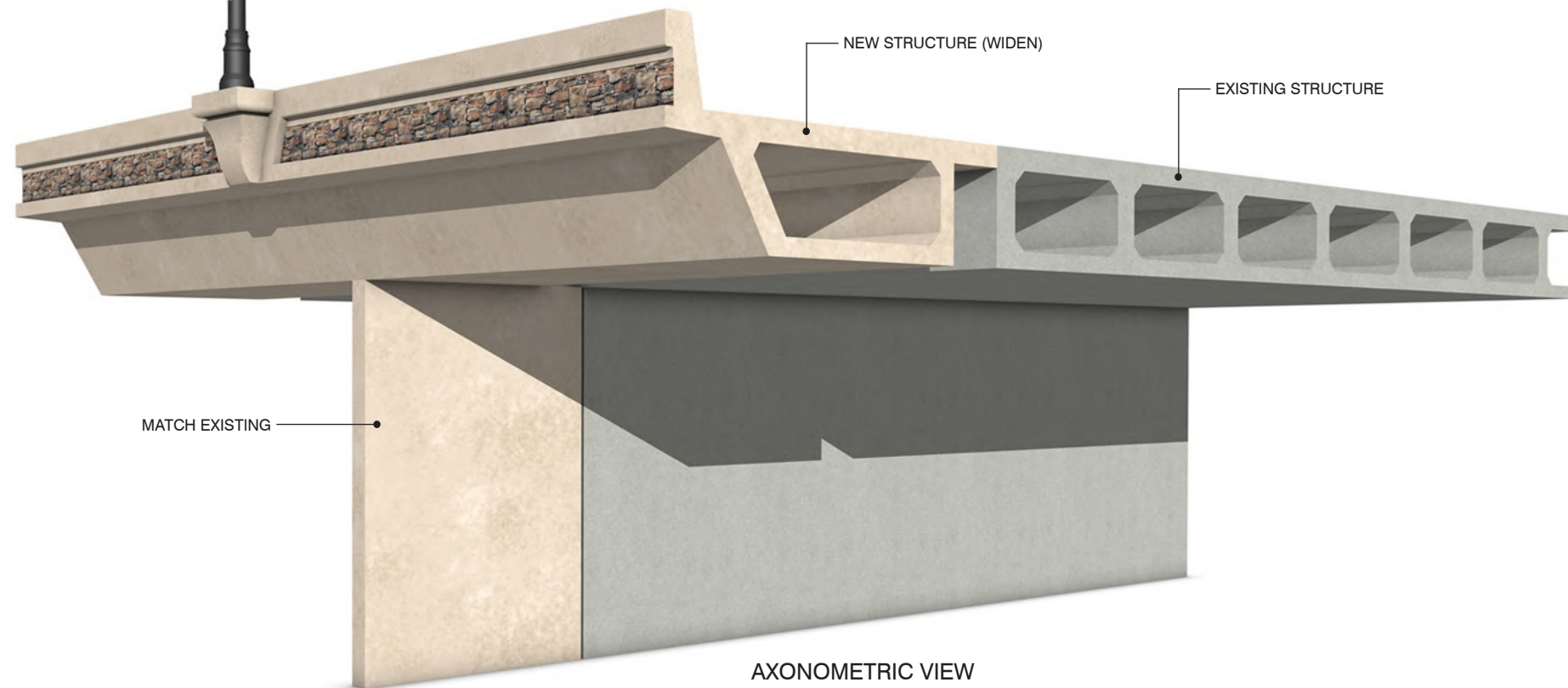
The term “bridge” is usually reserved for structures over water courses. Existing bridges along the I-605 Corridor will be widened. The “Aesthetic Committee Master Plan Theme” will be applied to the widened portions of the bridges to incorporate the design theme and unify the structures along the corridor.



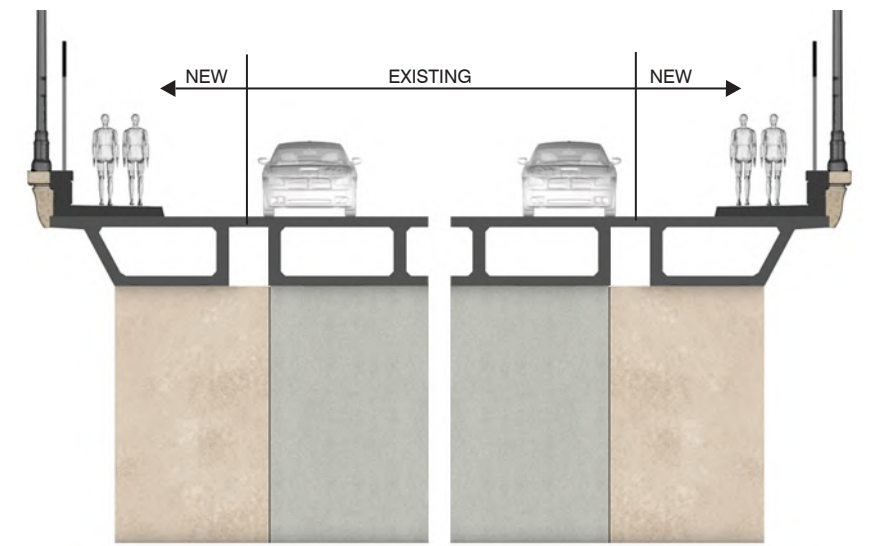
PERSPECTIVE VIEW



AXONOMETRIC VIEW



AXONOMETRIC VIEW

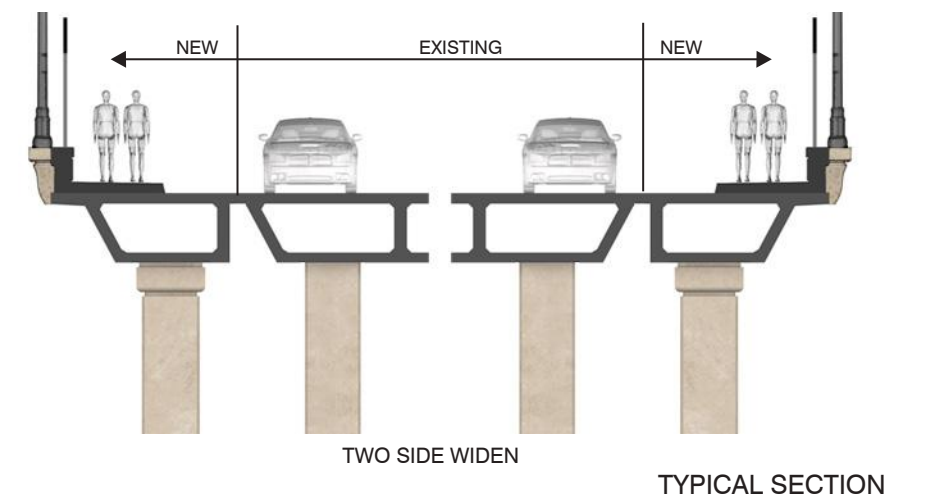
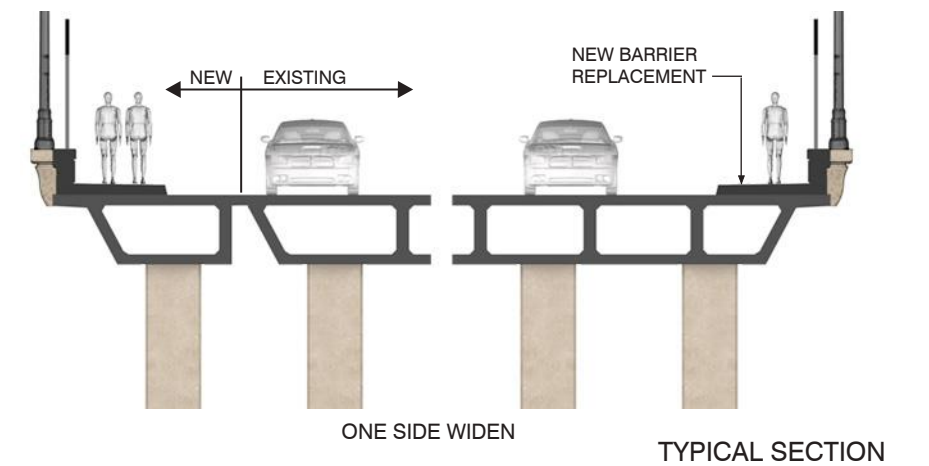
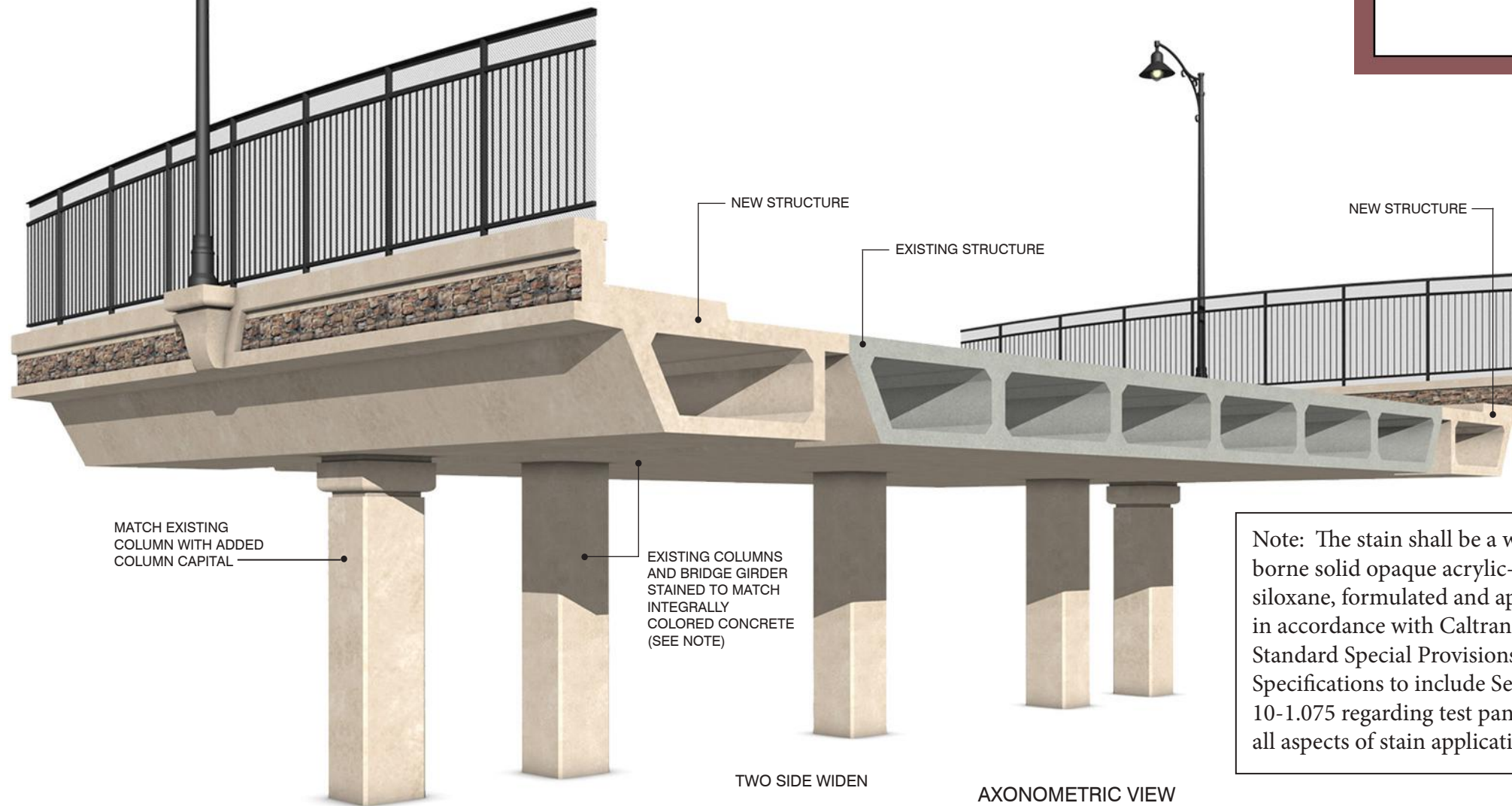
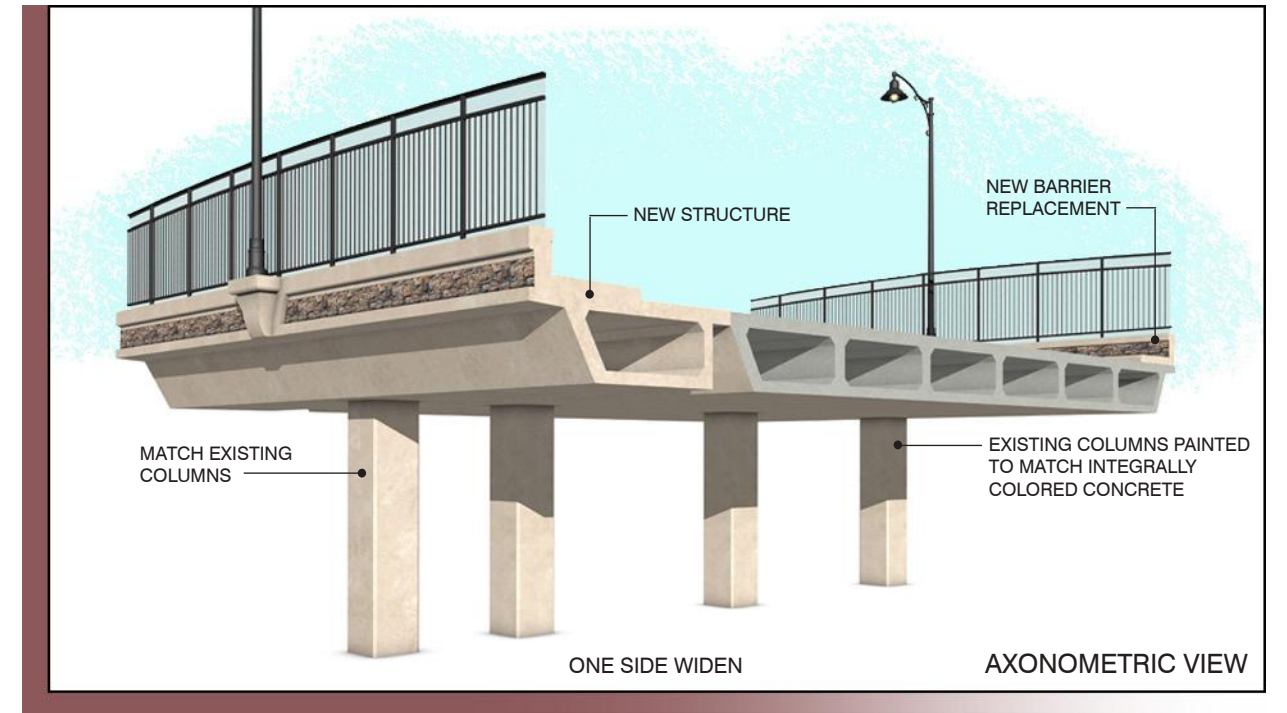
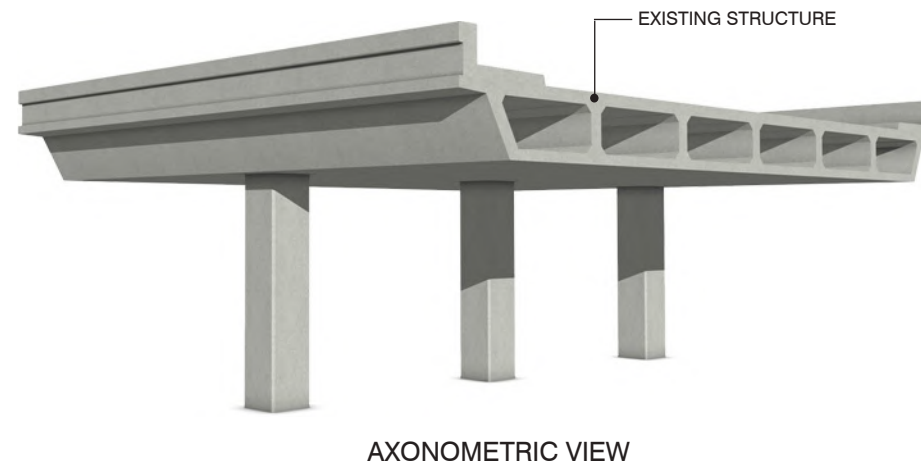


TYPICAL SECTION



#### 4.7 WIDENING EXISTING STRUCTURES

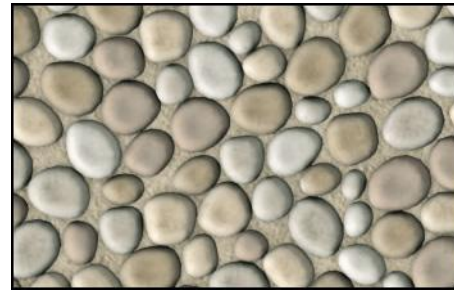
There will be several locations along the I-605 Corridor where the widening of existing structures will occur. The I-605 Project architectural treatment will be applied to the exterior girder, new bridge barriers, fencing, lighting, etc. to unify the existing structures with the new structures. At locations with one side widened, bridge columns will match the existing structure for visual consistency. At locations with both sides widened, bridge columns will match the existing structure with the addition of a capital.



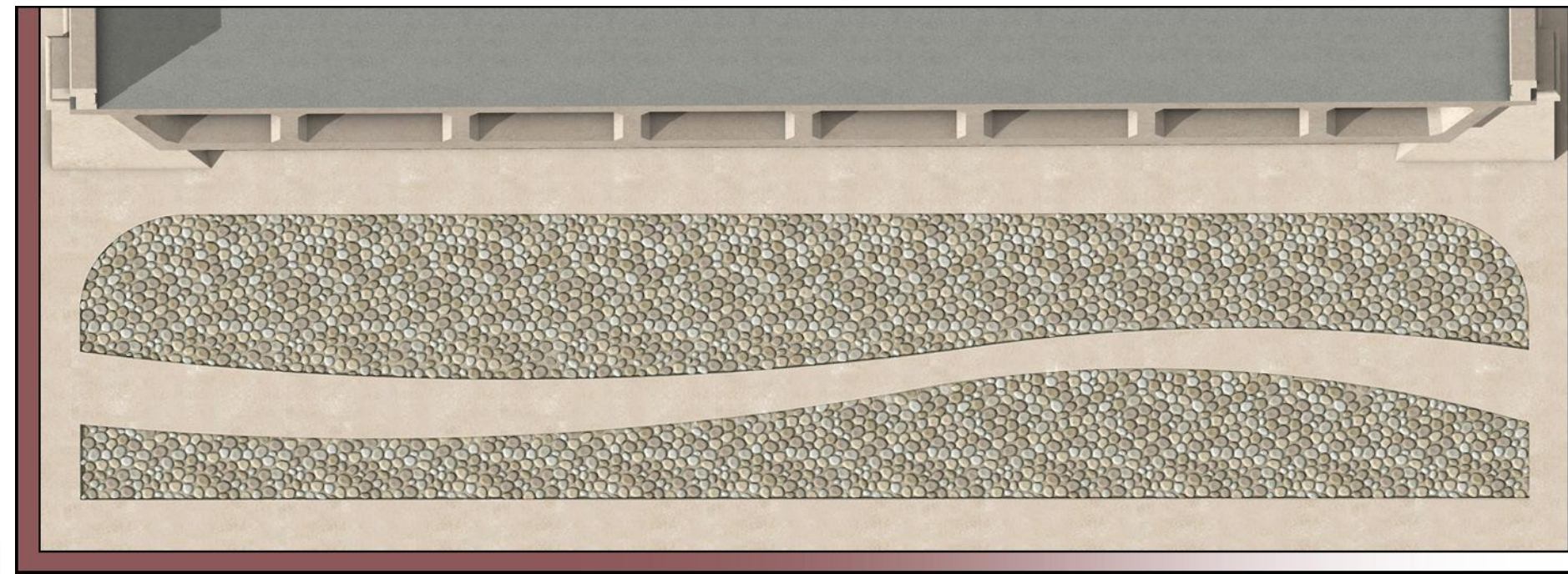
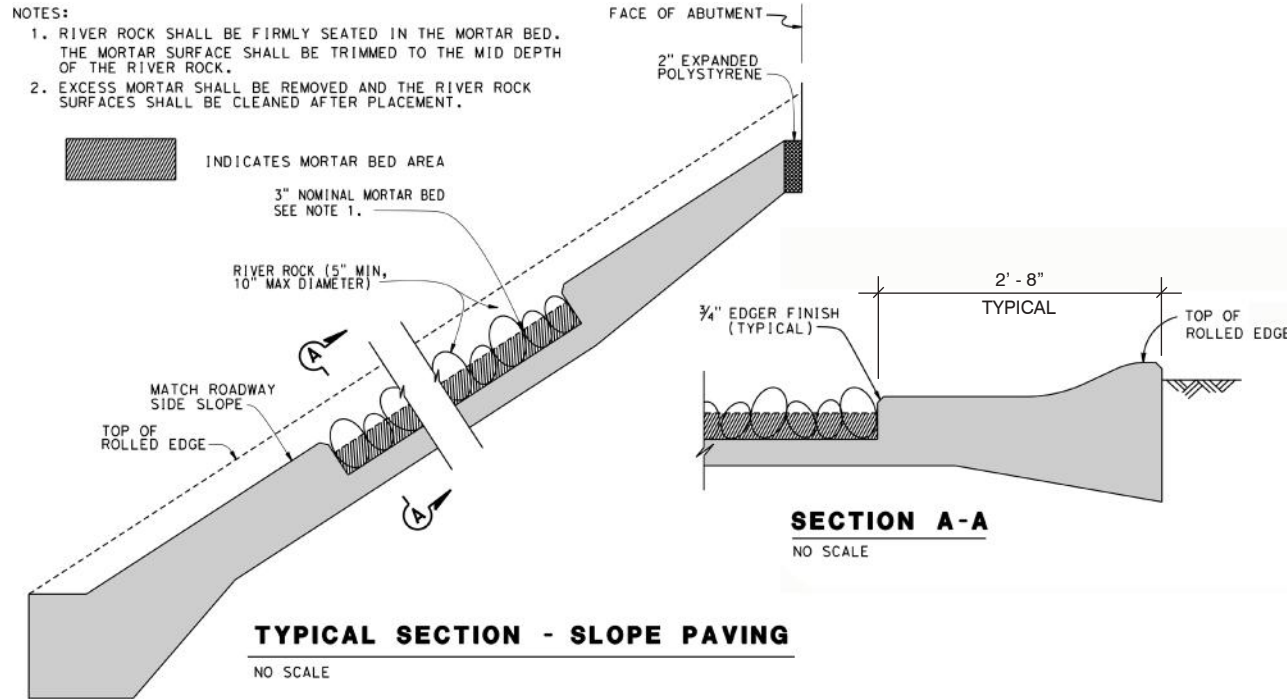
Note: The stain shall be a water-borne solid opaque acrylic-siloxane, formulated and applied in accordance with Caltrans' Standard Special Provisions and Specifications to include Section 10-1.075 regarding test panels and all aspects of stain application.

#### 4.8 SLOPE PAVING

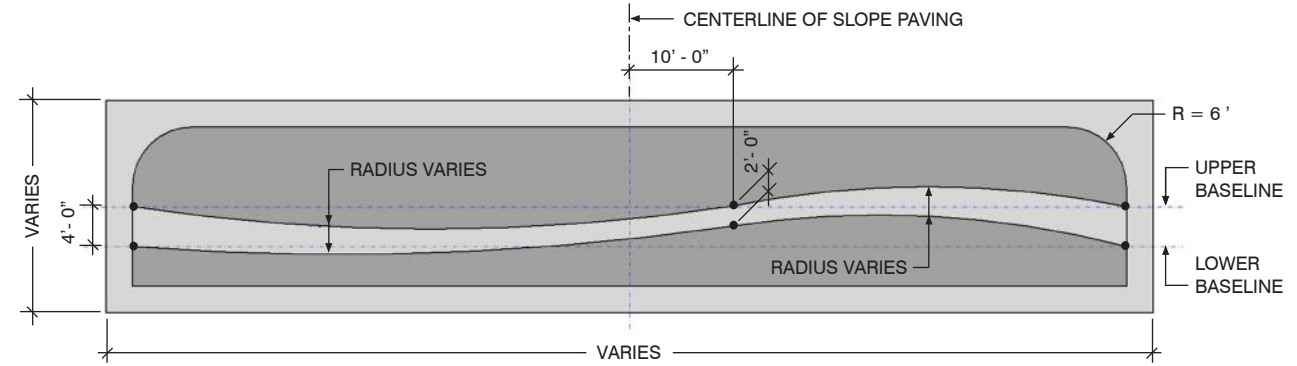
Slope paving will incorporate the “Aesthetics Committee Master Plan Theme”. This will consist of integrally colored concrete, multi-colored river rock in mortar, with a concrete band flowing horizontally through the rock bed.



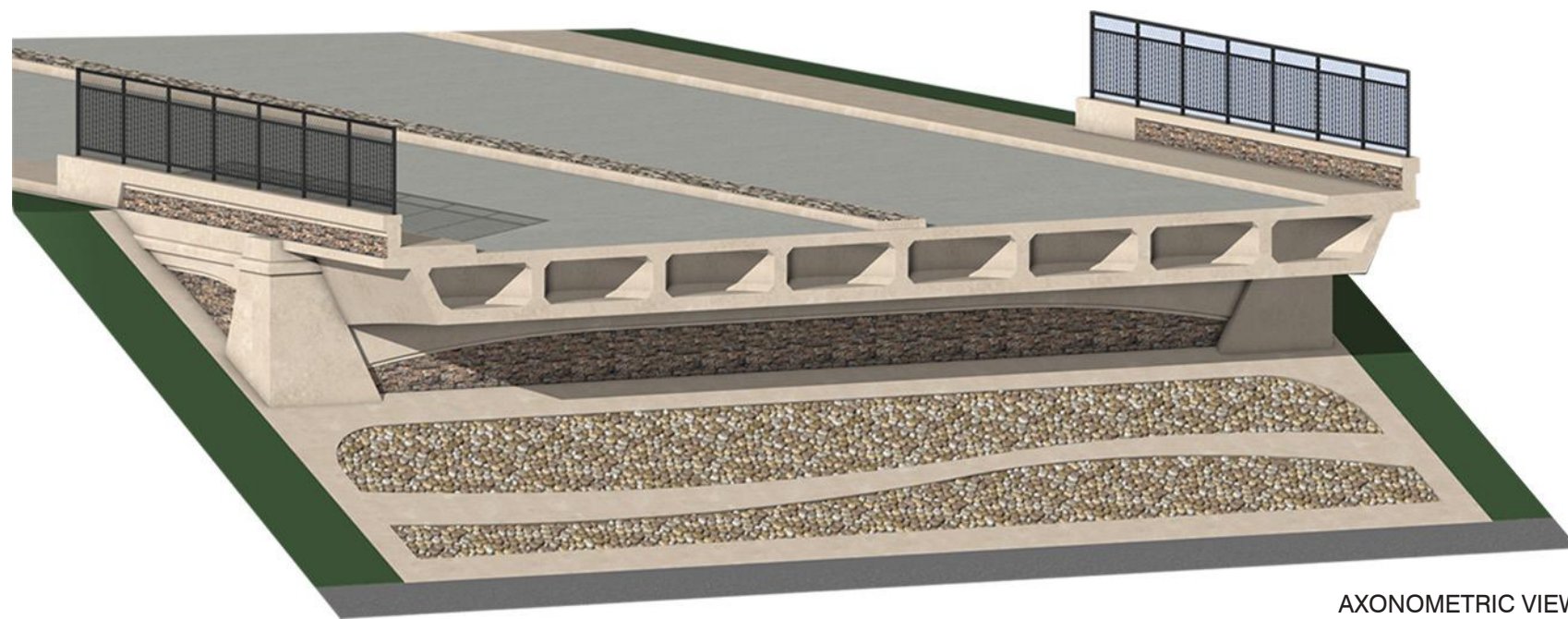
RIVER ROCK



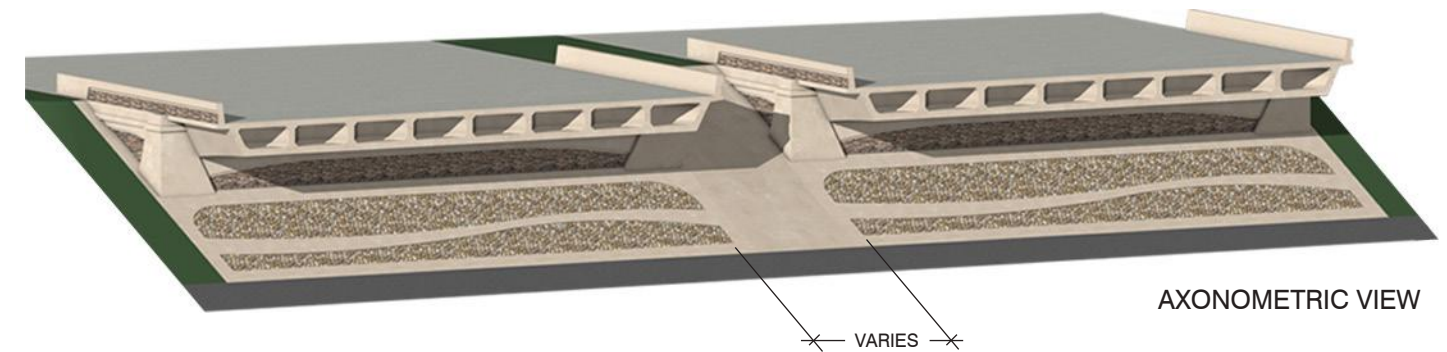
PLAN VIEW



Slope paving improves the overall appearance of the bridge. Used properly, slope paving is an integral element of bridge design which ties together the bridge structure and the surrounding roadway and visually anchors the structure to the environment.



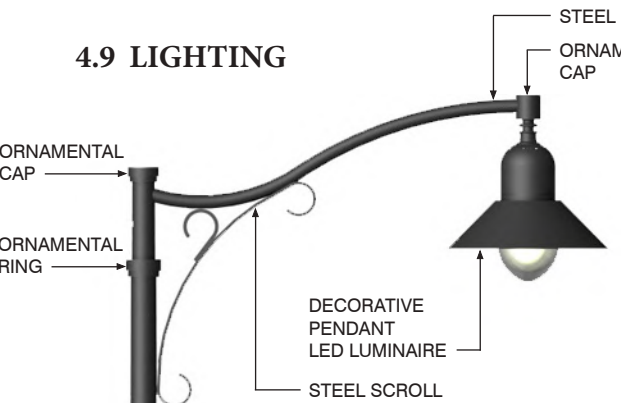
AXONOMETRIC VIEW



AXONOMETRIC VIEW

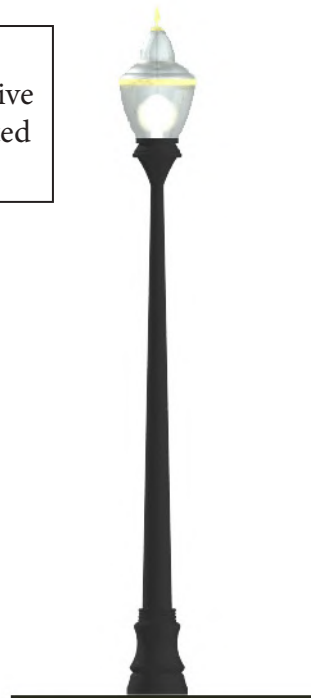
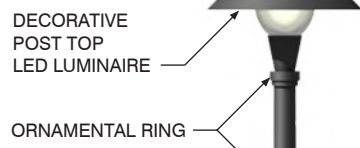
FINAL

4.9 LIGHTING



Lighting is an important feature for a bridge structure both in its functionality and appearance. When selecting lighting fixtures, it is important to not only enhance the design of the structure, but also to compliment the existing lighting from the surrounding areas. For the I-605 CMP, a family of lighting fixtures have been proposed that are simple and elegant in design, yet similar to existing lighting along the I-605 corridor to unify the communities. City to negotiate new street lighting.

Note: All light post components to receive a black powder coated finish



STREET LIGHTING

STRUCTURAL LIGHTING



PERSPECTIVE VIEW

FINAL

**4.10 RETAINING WALLS**

All new retaining walls throughout the I-605 Corridor will have consistent architectural treatment. The textures and patterns will be consistently used on all wall types, whether they are MSE, cast in place, or other wall types.

The “Aesthetics Committee Master Plan Theme” will be applied to all retaining walls and will be a combination of: integrally colored concrete, painted drystack stone texture, basalt texture, and in featured or taller walls a colored horizontal “river” band.

Longer retaining walls may have vertical breaks to minimize monotony and add visual interest.



**DRystack TEXTURE**

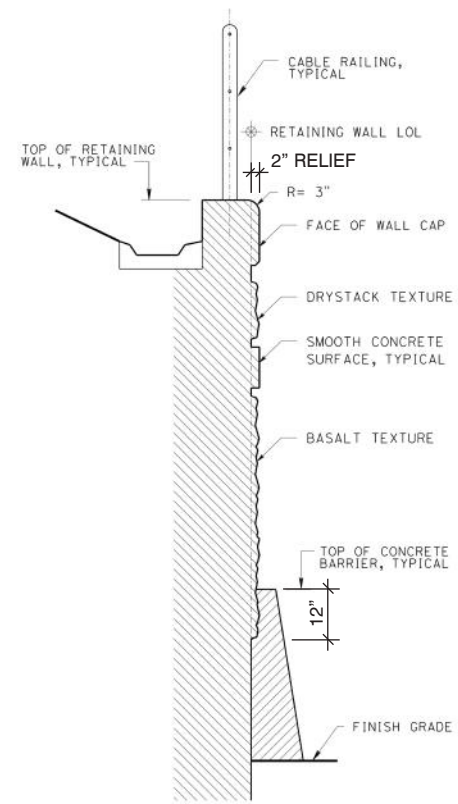
TO MATCH FITZGERALD PATTERN #17911 - 1 1/2" RELIEF  
SAN DIEGO DRystack TEXTURE  
PAINT COLORS TO MATCH #'s 30117, 30215, 36251, AND 37722 OF FED-STD-595



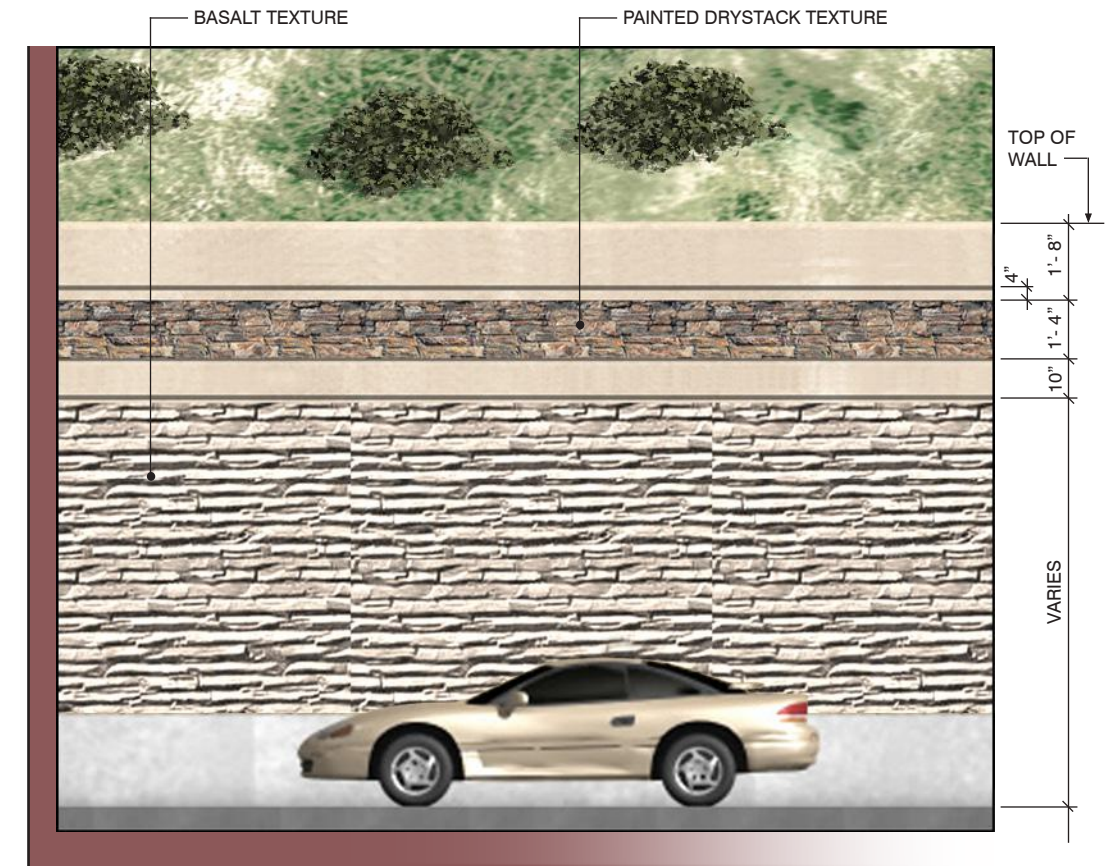
**BASALT TEXTURE**

TO MATCH FITZGERALD PATTERN #17928 - 2" RELIEF  
WESTERN BASALT TEXTURE (HORIZONTAL ORIENTATION)

Note: Cable Railings have been eliminated from renderings for clarity purposes.



**TYPICAL SECTION**  
NO SCALE



**PARTIAL ELEVATION VIEW**



**BIRD'S EYE - PERSPECTIVE VIEW**

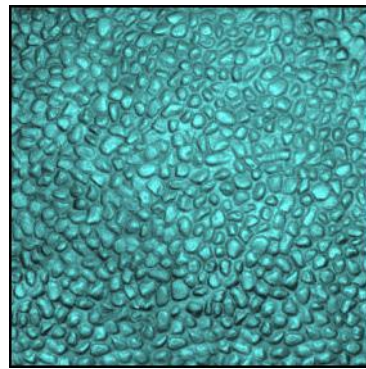
**FINAL**

**4.10 RETAINING WALLS**

Along the I-605 Corridor, a majority of new retaining walls will be seen from adjacent communities. Where right-of-way space allows maintenance access, vegetation can be planted to “soften” wall surfaces. The Contractor will construct vine planting cutout areas at the base of retaining walls. The vines will eventually grow and extend above the sound wall and will be seen from the freeway side.



**DRystack TEXTURE**  
TO MATCH FITZGERALD PATTERN #17911 - 1 1/2" RELIEF  
SAN DIEGO DRystack TEXTURE PAINT COLORS TO MATCH #'s 30117, 30215, 36251, AND 37722 OF FED-STD-595



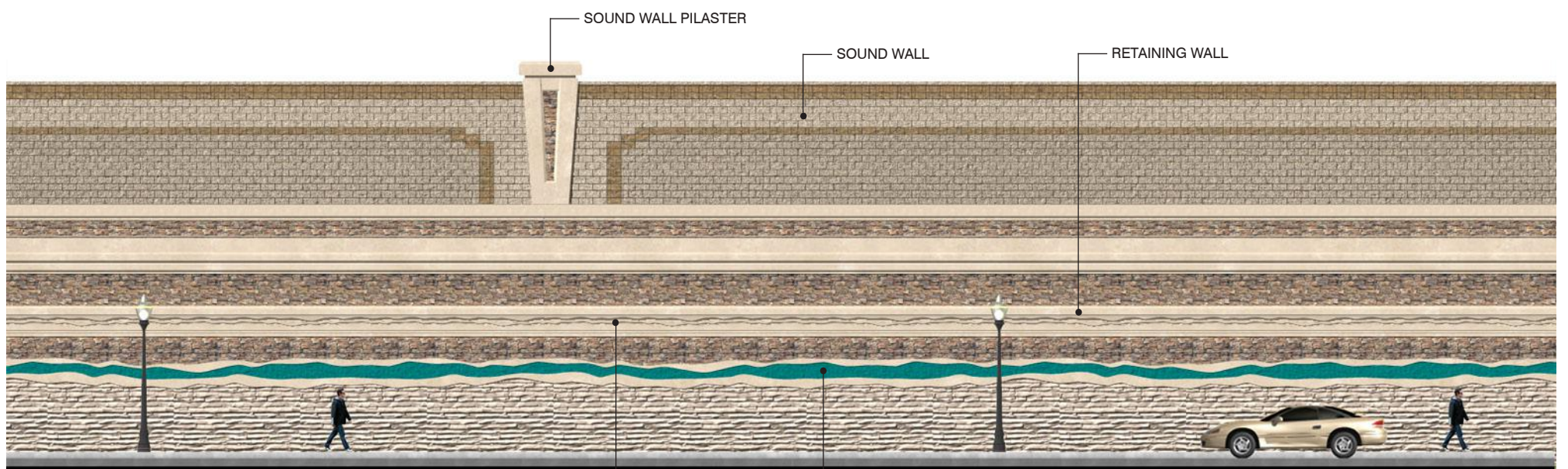
**STONE TEXTURE**  
TO MATCH FITZGERALD PATTERN #16983 - 3/8" RELIEF ROUNDSTONE  
TEXTURE PAINT COLOR TO MATCH #35193 OF FED-STD-595



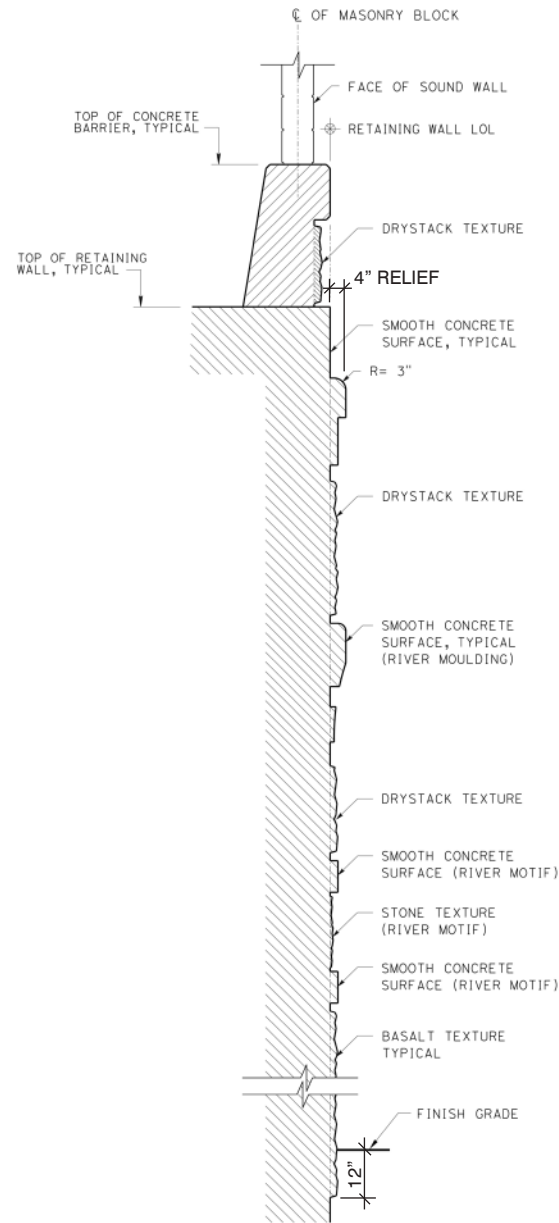
**BASALT TEXTURE**  
TO MATCH FITZGERALD PATTERN #17928 - 2" RELIEF  
WESTERN BASALT TEXTURE (HORIZONTAL ORIENTATION)



PERSPECTIVE VIEW



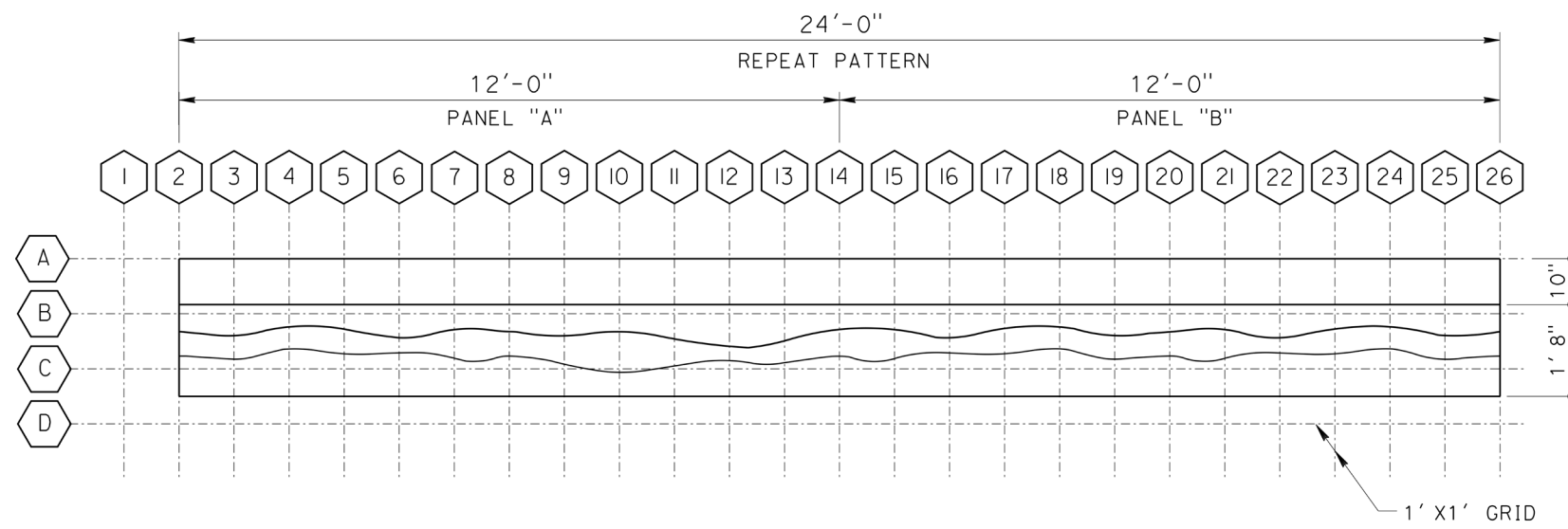
PARTIAL ELEVATION VIEW



**TYPICAL SECTION**  
NO SCALE

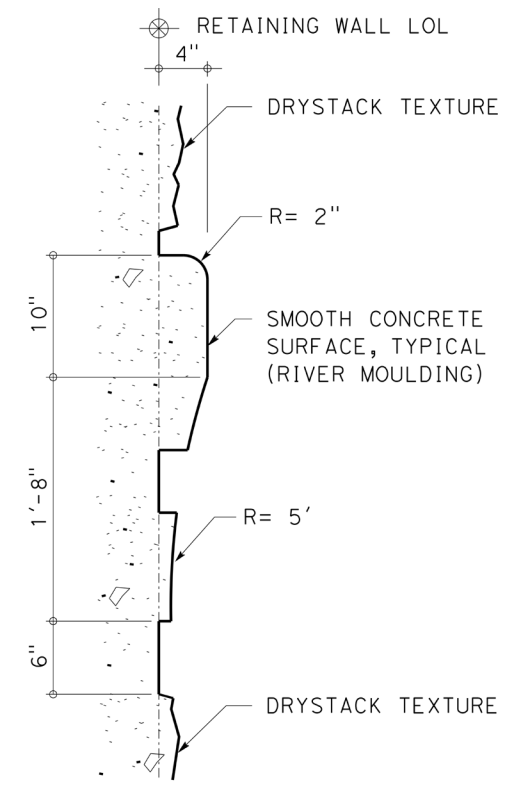
FINAL

4.11 RETAINING WALL DETAILS



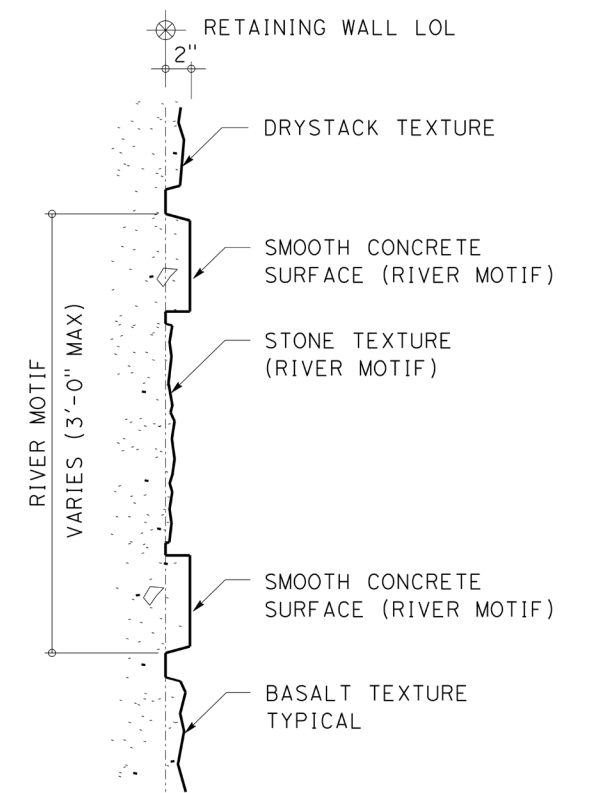
**ELEVATION - RIVER MOULDING PATTERN**

NO SCALE



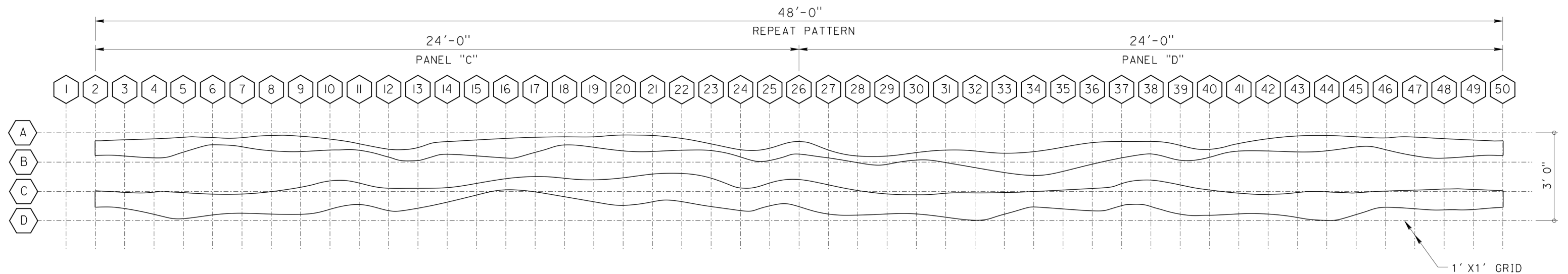
**RIVER MOULDING DETAIL**

NO SCALE



**RIVER MOTIF DETAIL**

NO SCALE



**ELEVATION - RIVER MOTIF PATTERN**

NO SCALE

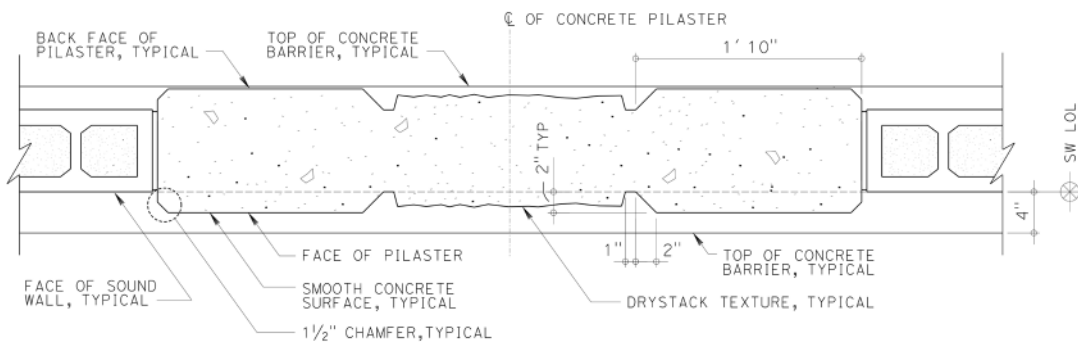
FINAL

### 4.12 SOUND WALLS

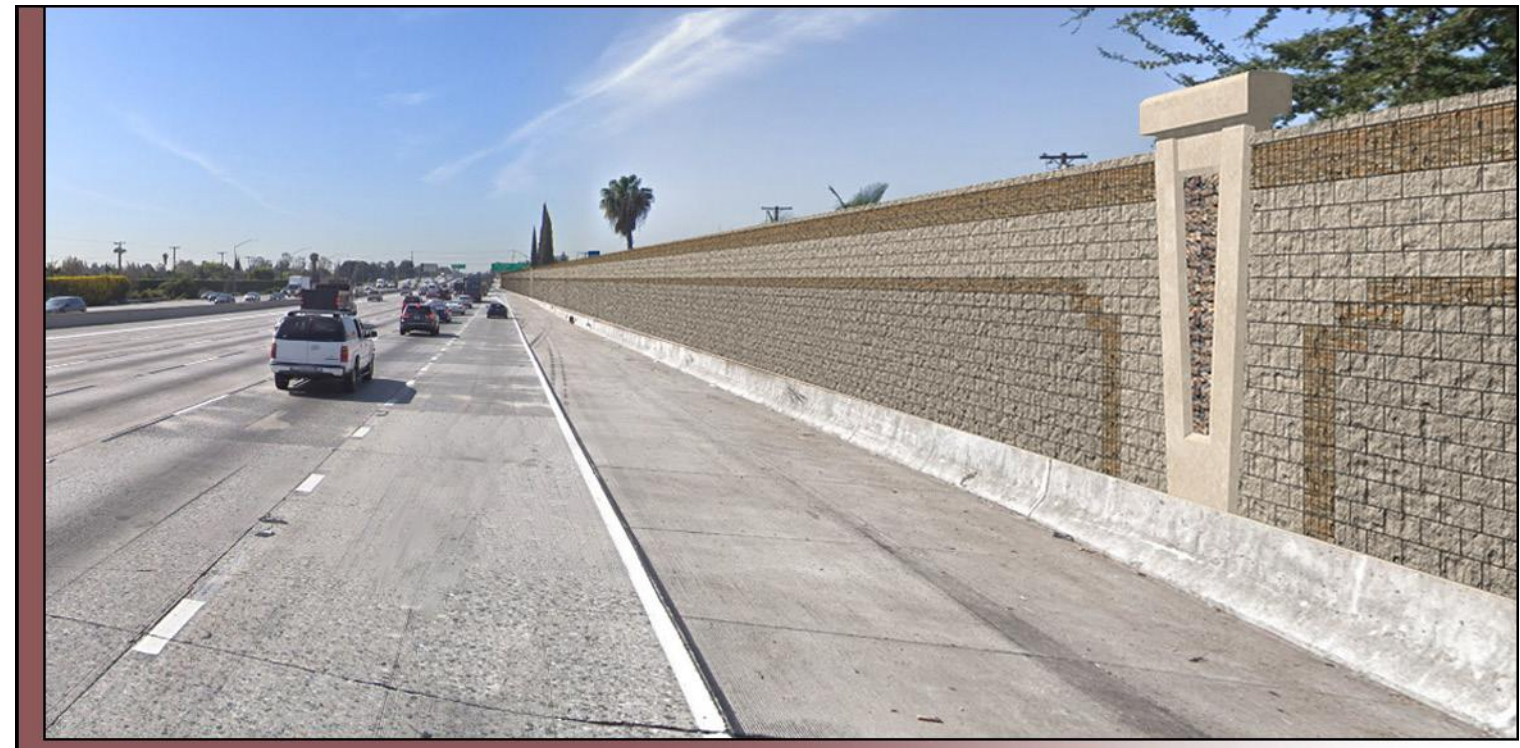
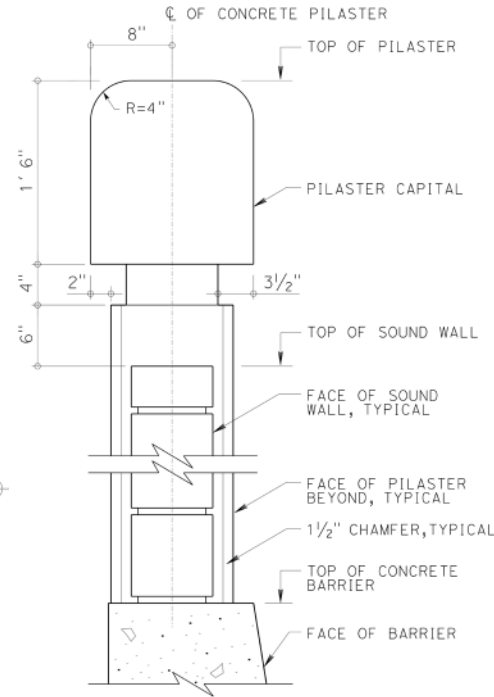
Sound walls for the I-605 corridor shall consist of integrally colored split face block in 3 different colors. Custom pilasters will be placed at regular intervals of 349'-4". The block motif is designed to incorporate placement of the pilaster. Sound wall heights will vary throughout the corridor and all design elements will adjust accordingly. Where appropriate, new sound walls will tie into existing sound walls. All pilasters shall be constructed vertical to profile grade (plumb).

NOMINAL SIZE	TEXTURE	COLOR
8" x 8" x 16"	SPLIT FACE 2 SIDES	NATURAL GRAY *
8" x 8" x 16"	SCORED SPLIT FACE 2 SIDES	OAK *
8" x 8" x 16"	SPLIT FACE 2 SIDES	PLACER CREEK *
4" x 8" x 16"	SPLIT FACE 2 SIDES CAP	NATURAL GRAY *

\* TO MATCH ANGELUS BLOCK COLORS



TYPICAL PILASTER SECTIONS



PERSPECTIVE VIEW



NATURAL GRAY



PLACER CREEK



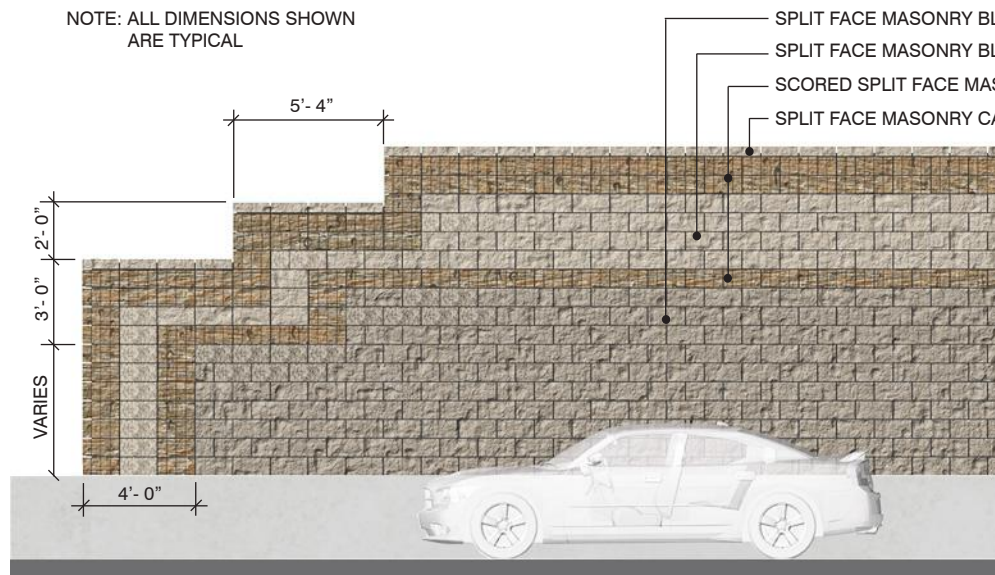
OAK

349'-4" ON CENTER (TYPICAL PILASTER SPACING)

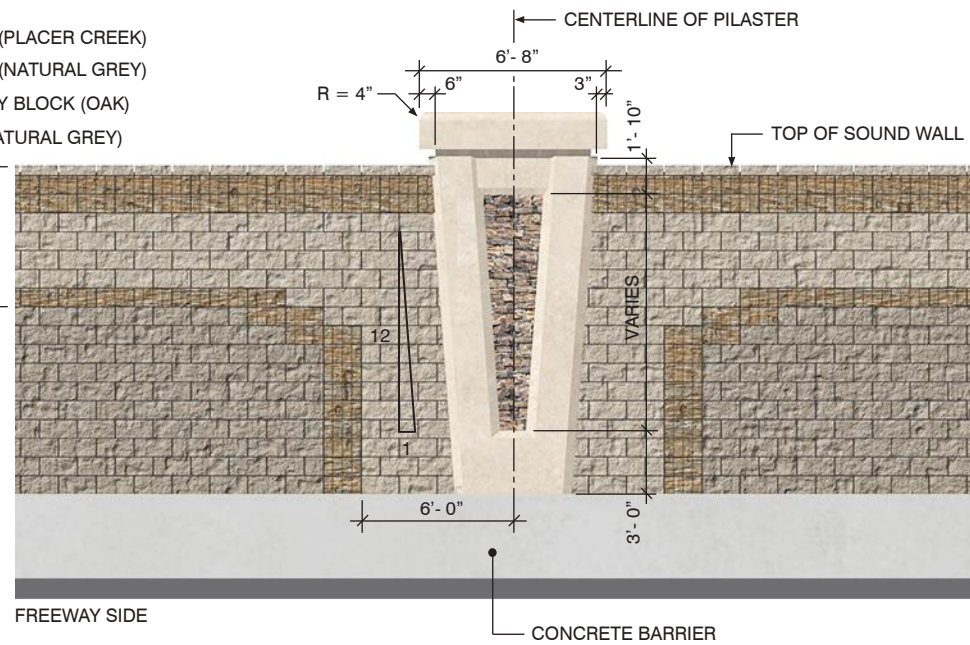


ELEVATION VIEW

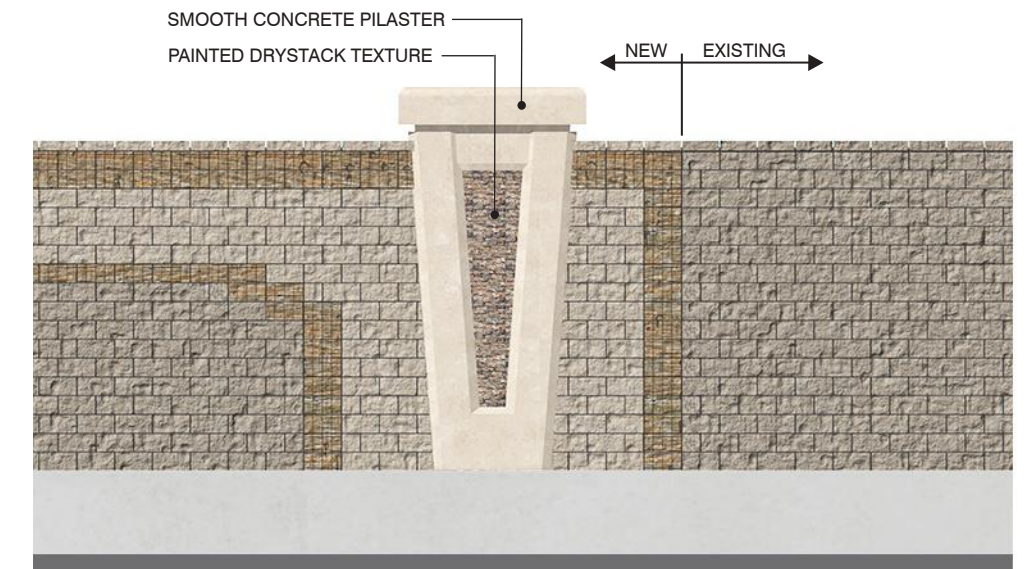
NOTE: ALL DIMENSIONS SHOWN ARE TYPICAL



END OF SOUND WALL



FREEWAY SIDE



NON - FREEWAY SIDE

PARTIAL ELEVATION VIEW

FINAL

## 5.0 LANDSCAPE

### 5.1 Plant Selection Process

Selecting plants for the I-605 Corridor was a long process which began years ago. Early freeway landscapes were a cornucopia of plants from all over the world. Large areas of freeway daisies and towering eucalyptus trees once dominated the roadside landscape. While these plants provide ample aesthetics and longevity, pests and diseases eventually took a toll on many of these non-native plants. In the ensuing years what has emerged is a recognition of the importance of native plants to sustain the roadside landscape. The work in selecting appropriate native plants continues as not all-natural plants are conducive to harsh roadside environments.

### 5.2 The Era of Regionally Appropriate Plants and Pollinators

In recent times the loss of habitat to urbanization has contributed to a decline in wild bees and butterfly populations. There has been a renewed interest in helping to sustain these pollinator populations. While the roadside landscape is a small fragmented habitat much can be done to improve the population of these pollinators. To help promote a roadside landscape that will attract to help the monarch butterflies, honeybees and hummingbirds, the new direction of freeway landscape design is to plant a pollinator-friendly roadside landscaping. This focus on pollinator-plant design is being done with regionally appropriate plants and native plants.

### 5.3 Plants as Design Elements

The plants selected for the I-605 Corridor will complement the new design of structures. Worker safety and long-term longevity of the plants is crucial to the success of roadside landscaping.

### 5.4 Ground Plane

To fill the ground plane low growing Prostrate Acacia was selected for drought tolerant, aesthetic appearance and ease of maintenance. When fully grown this plant will reduce worker exposure along the roadside.

### 5.5 Vertical Space

To accentuate the vertical space of the freeway several trees were selected, Western Redbud, California Sycamore, and Coast Live Oak. All the trees are California natives.

To add accent at critical points such as off-ramps or on-ramps, San Diego Red Bougainvillea and Spreading Yellow Lantana are used.

To lure beneficial insects to the roadside landscape Narrowleaf Milkweed and California Flannelbush will be planted in select areas.

### 5.6 Wall Screening

To screen walls and undesirable views a mass of Sugar Bush will add a rich, deep green background. Imperial Blue Plumbago will be used to accent the massing of Sugar Bush.

The Sound Walls will be aesthetically pleasing, and only minimal planting of vines will accent the wall features. No plantings of vines near pilasters, this is done to preserve their aesthetic character. Boston ivy will add a light greenery to wall and will be accented with the addition of Red Trumpet Vine.

### 5.7 Stormwater Requirements

Methods typically utilized to improve the quality of the water run-off adjacent project pavement includes detention ponds that allow pollutants to settle out and bioswales, which are earth ditches that use grass plantings along the swale to filter out the sediments and slow down water run-off.

Deer Grass and forbs are used in bioswales to give a distinct native character and along the perimeter showy Foothill Penstemon will accent.

California Sycamores, Bougainvillea, and California Flannelbush shall be utilized along the edge to provide aesthetic screening for the basins.

### 5.8 Drought Years

The greater Los Angeles area has a history of droughts. One drought occurred in the mid- 1970's by then the Los Angeles area freeways were lush with high water use plants. The freeway plant oasis was soon met with the reality of severe water restrictions. Caltrans District 7 recognized water use needed to be curtailed while keeping greenery along the freeway alive, motorists by then was accustomed to a sea of greenery along LA Basin freeways.

A team of Landscape Architects searched arboretums and consulted with horticulturist on plants that were drought tolerant. Many plants selected came from the continent of Australasia and the country of South Africa. These plants performed as expected and in the ensuing years reduced water consumption.

Regardless of the abundant rain received in the recent years Caltrans District 7 continues to be responsible with water usage

### 5.9 Early Recycled Water Use

Since the mid-1970's southern California region has endured three more droughts, the last one was the drought of 2012-2017. The Los Angeles area has seen a surge in population growth since the mid-1970 and the demand for scarce water supplies has grown as well. District-7 has recognized water is a precious commodity and since the 1970s has used recycled water for freeway landscaping when available. The quest to reduce water consumption for freeway landscapes continues to this day.

If recycled water opportunities arise during the design phase of the I-605 Corridor projects, recycled water will be used to the maximum extent possible, to reduce the need for potable water.



**Trees**



*Cercis orbiculata*  
Redbud



*Platanus racemosa*  
California Sycamore



*Quercus agrifolia*  
Coast Live Oak

**Shrubs**



*Acacia redolens* 'Desert Carpet'  
Prostate Acacia



*Asclepis fascicularis*  
Narrowleaf Milkweed

**Shrubs**



*Bougainvillea* 'San Diego Red'  
San Diego Red Bougainvillea



*Fremontodendron californicum*  
California Flannelbush



*Lantana* 'Spreading Yellow'  
Spreading Yellow Lantana



*Penstemon heterophyllus*  
Foothill Penstemon



*Plumbago auriculata* 'Imperial Blue'  
Imperial Blue

**Shrubs**



*Rhus ovata*  
Sugar Bush



*Salvia clevelandii*  
Cleveland Sage

**Vines**



*Distictis buccinatoria*  
Red Trumpet Vine



*Parthenocissus tricuspidata*  
Boston Ivy

**Ground cover**



*Muhlenbergia rigens*  
Deer Grass

# APPROVAL

## Gateway Cities, Council of Governments

City	Representative	Signature
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## San Gabriel Valley, Council of Governments

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## Los Angeles Transportation Authority (LA Metro)

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## California Department of Transportation (Caltrans)

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DATE: September 16, 2019

TO: Public Works Technical Advisory Committee

FROM: Marisa Creter, Executive Director

RE: **LACMTA TECHNICAL ADVISORY COMMITTEE REPRESENTATIVE APPOINTMENT**

## **RECOMMENDED ACTION**

Discuss and provide direction to staff.

## **BACKGROUND**

The Los Angeles County Metropolitan Transportation Authority (Metro) established a Technical Advisory Committee (TAC) in 1977 in accordance to state law. The TAC provides technical assistance to Metro by reviewing and evaluating the various transportation proposals and alternatives within Los Angeles County. Issues and topics discussed by the TAC include the funding, operation, construction, and maintenance of streets and freeways, bus and rail transit projects, demand and system management initiatives, accessibility projects for individuals facing disabilities, and air quality improvement projects. The committee reviews and provides recommendations on these issues and topics to the Metro Board of Directors and Metro staff members.

Additionally, the TAC consists of four standing subcommittees:


- **Bus Operations Subcommittee:** Reviews and provides technical input on all bus operations, capital, and legislative issues.
- **Streets and Freeways Subcommittee:** Reviews and provides technical input on projects and issues related to seaports and airports access, goods movement, bicycle improvements, pedestrian improvements, transportation enhancements activities, traffic systems management, and streets and freeways operation and programming.
- **Local Transit Systems Subcommittee:** Provides technical input on issues affecting local transportation systems, including fixed route circulation, commuter services, paratransit, and rail feeder services.
- **Transportation Demand Management (TDM)/Sustainability Subcommittee:** Provides technical input on TDM, air quality issues, bicycle and pedestrian improvements, transportation enhancement activities, and Metro programs that assist the implementation of a countywide transportation system to increase mobility, foster walkable and livable communities, and minimize greenhouse gas and environmental impacts.


The TAC is comprised of 30 voting members and five ex-officio members and meets on the first Wednesday of every month at 9:30am at the Metro Headquarters Building at 1 Gateway Plaza, Los Angeles, CA 90012. One of the 30 voting representatives must be appointed by the San Gabriel Valley Council of Governments (SGVCOG).

The appointed representative must be a full-time employee from one of SGVCOG's member cities and cannot be a paid consultant. After being appointed by the SGVCOG, the representative must also be confirmed by the League of California Cities in order to serve as an official voting member of the TAC. The membership structure of the TAC can be found in Attachment A.

Mr. Larry Stevens, who served as the SGVCOG's representative to the Metro TAC and the Assistant City Manager of the City of San Dimas, recently retired from the City. Given his retirement, the SGVCOG Public Works Technical Advisory Committee was provided with the opportunity to appoint a committee member to serve as the SGVCOG representative on the Metro TAC.

SGVCOG Management Analyst, Alexander Fung, will provide a brief presentation on the background and duties of the Metro TAC at this meeting.

Prepared by:   
\_\_\_\_\_  
Alexander P. Fung  
Management Analyst

Approved by:   
\_\_\_\_\_  
Marisa Creter  
Executive Director

## **ATTACHMENTS**

Attachment A – TAC Membership Structure

## TECHNICAL ADVISORY COMMITTEE

### MEMBERSHIP STRUCTURE

<u>AGENCY</u>	<u>MEMBERS</u>
City of Los Angeles	3
County of Los Angeles	3
League of Cities	8
Caltrans	2
Metro Operations	1
Bus Operations Subcommittee (BOS)	2
City of Long Beach	1
Local Transit Systems Subcommittee (LTSS)	2
Metro Chief Executive Officer or Alternate	1
Citizen Representative on ADA Issues	1
Southern California Regional Rail Authority (SCRRA)*	1
Southern California Association of Governments (SCAG)*	1
Southern California Air Quality Management District (SCAQMD)*	1
Southern California Automobile Club	1
California Highway Patrol (CHP)	1
Goods Movement Representative*	1
Transportation Demand Management/Air Quality Subcommittee (TDM/AQ)	2
Bicycle Coordinator	1
Pedestrian Coordinator	1
Public Health Representative*	1
Total	35

35 members, 30 voting members

\*ex-officio (non-voting) member