



San Gabriel Valley Council of Governments
AGENDA AND NOTICE OF THE SPECIAL MEETING OF THE
TRANSPORTATION COMMITTEE – OCTOBER 24, 2019 – 3:30 PM
FOOTHILL TRANSIT OFFICE
100 S. VINCENT AVE., SUITE 200, WEST COVINA, CA

Chair
John Fasana, Duarte

Vice-Chair
Jason Pu, San Gabriel

Members
Alhambra
Claremont
Diamond Bar
Duarte
Glendora
Industry
Monterey Park
Pomona
San Gabriel
South El Monte
South Pasadena
Temple City
Walnut
First District, LA County
Unincorporated
Communities
Fifth District, LA County
Unincorporated
Communities

The Transportation Committee encourages public participation and invites you to share your views on agenda items.

MEETINGS: *Regular Meetings of the Transportation Committee are held on the third Thursday of each month at 3:00 PM at the City of Duarte Community Center (1600 E. Huntington Drive, Suite B, Duarte, California, 91010).* The Transportation Committee agenda packet is available at the San Gabriel Valley Council of Government's (SGVCOG) Office, 1000 South Fremont Avenue, Suite 10210, Alhambra, CA, and on the website, www.sgvkog.org. Copies are available via email upon request (sgv@sgvcog.org). Documents distributed to a majority of the Committee after the posting will be available for review in the SGVCOG office and on the SGVCOG website. Your attendance at this public meeting may result in the recording of your voice.

CITIZEN PARTICIPATION: Your participation is welcomed and invited at all Transportation Committee meetings. Time is reserved at each regular meeting for those who wish to address the Committee. SGVCOG requests that persons addressing the Committee refrain from making personal, slanderous, profane, or disruptive remarks.

TO ADDRESS THE TRANSPORTATION COMMITTEE: At a regular meeting, the public may comment on any matter within the jurisdiction of the Committee during the public comment period and may also comment on any agenda item at the time it is discussed. At a special meeting, the public may only comment on items that are on the agenda. Members of the public wishing to speak are asked to complete a comment card or simply rise to be recognized when the Chair asks for public comments to speak. We ask that members of the public state their name for the record and keep their remarks brief. If several persons wish to address the Committee on a single item, the Chair may impose a time limit on individual remarks at the beginning of discussion. **The Transportation Committee may not discuss or vote on items not on the agenda.**

AGENDA ITEMS: The Agenda contains the regular order of business of the Transportation Committee. Items on the Agenda have generally been reviewed and investigated by the staff in advance of the meeting so that the Transportation Committee can be fully informed about a matter before making its decision.

CONSENT CALENDAR: Items listed on the Consent Calendar are considered to be routine and will be acted upon by one motion. There will be no separate discussion on these items unless a Committee member or citizen so requests. In this event, the item will be removed from the Consent Calendar and considered after the Consent Calendar. If you would like an item on the Consent Calendar discussed, simply tell Staff or a member of the Committee.



In compliance with the Americans with Disabilities Act, if you need special assistance to participate in this meeting, please contact the SGVCOG office at (626) 457-1800. Notification 48 hours prior to the meeting will enable the SGVCOG to make reasonable arrangement to ensure accessibility to this meeting.



PRELIMINARY BUSINESS

1. Call to Order
2. Pledge of Allegiance
3. Roll Call
4. Public Comment (*If necessary, the Chair may place reasonable time limits on all comments*)
5. Changes to Agenda Order: Identify emergency items arising after agenda posting and requiring action prior to next regular meeting

CONSENT CALENDAR (*It is anticipated that the Transportation Committee may take action on the following matters*)

6. Transportation Committee Meeting Minutes – 09/19/2019
Recommended Action: Approve Transportation Committee minutes.

ACTION ITEMS (*It is anticipated that the Transportation Committee may take action on the following matters*)

PRESENTATIONS (*It is anticipated that the Transportation Committee may take action on the following matters*)

7. SB 743 Implementation– Danny Wu, Iteris – Page 5
Recommended Action: For information only.
8. I-605 Corridor Aesthetics Master Plan – Mark Christoffels, SGVCOG – Page 7
Recommended Action: Recommend that the Governing Board approve the I-605 Corridor Aesthetics Master Plan.

DISCUSSION ITEMS (*It is anticipated that the Transportation Committee may take action on the following matters*)

METROPOLITAN TRANSPORTATION AUTHORITY (MTA) REPORT (*It is anticipated that the Transportation Committee may take action on the following matters*)

9. Oral Report
Recommended Action: For information only.

UPDATE ITEMS

10. Metrolink Update
Recommended Action: For information only.
11. Gold Line Update
Recommended Action: For information only.

EXECUTIVE DIRECTOR’S REPORT (*It is anticipated that the Transportation Committee may take action on the following matters*)

12. Oral Report
Recommended Action: For information only.

COMMITTEE MEMBER ITEMS

ANNOUNCEMENTS

ADJOURN



SGVCOG Transportation Committee Approved Minutes

Date: September 19, 2019

Time: 3:00 PM

Location: City of Duarte Community Center: Conference Room
1600 E. Huntington Drive; Duarte, California 91010

PRELIMINARY BUSINESS

1. Call to Order

The meeting was called to order at 3:10 p.m.

2. Pledge of Allegiance

3. Roll Call

Members Present

Claremont	E. Reece
Diamond Bar	D. Liu
Duarte	J. Fasana
Glendora	S. Mateer
LA County District 1	M. Reyes
Monterey Park	P. Chan
San Gabriel	J. Pu
Temple City	A. Avery

Members Absent

Alhambra	J. Maloney
LA County District 5	D. Perry
Pomona	R. Guerrero
South El Monte	G. Olmos
South Pasadena	D. Mahmud
Walnut	A. Wu

Guests

Andrew Meyer
Andrew Ross
Corey Moss
Gregory Hatcher
Keith Snyder
Ritta Merza

SGVCOG Staff

N. Kaur

4. Public Comment

No general public comment.

5. Changes to Agenda Order: Identify emergency items arising after agenda posting and requiring action prior to next regular meeting

There were no changes to the agenda order.

CONSENT CALENDAR

6. Transportation Meeting Minutes: 09/19/2019

Recommended Action: Approve Transportation Committee minutes.

There was a motion made to approve the Consent Calendar. (M/S: M. Reyes/S. Mateer)

[MOTION PASSED]

AYES:	Claremont, Diamond Bar, Duarte, Glendora, LA County District 1, Monterey Park, San Gabriel, Temple City
NOES:	
ABSTAIN:	
ABSENT:	Alhambra, LA County District 5, Pomona, South El Monte, South Pasadena, Walnut

PRESENTATIONS

7. Ontario Airport Presentation

Ontario Airport staff presented on Ontario Airport’s standard operating procedures and the Airport’s voluntary noise mitigation procedures. The SGVCOG, as well as other entities, has been made aware of multiple flight noise complaints from residents in the surrounding areas. The SGVCOG has received several explanations for the flight noise, including the following:

- An increase in commercial flights;
- An increase in cargo flights (FedEx, UPS, DHL, Amazon); and
- Changes to flight patterns.

The COG has been assured that all operations are congruent with all FAA regulations and safety standards. Airport staff assured the Committee that there have been no changes in outbound and inbound flight patterns in decades. In fact, the quantity of flights has seen a decline on an annual basis. Residents who wish to notify Ontario Airport of flight complaints can visit www.flyontario.com. Residents will receive a response within one week. Ontario Airport has been and will continue to actively market flights, both domestic and international, in order to bring more business to the region and cheaper flight options for passengers.

Questions/Discussion:

- Ontario Airport has not changed outbound or inbound flights in roughly 30 years as they are established routes.
- Complaints could potentially be from newer residents who are not used to residing near an airport.

METROPOLITAN TRANSPORTATION AUTHORITY (MTA) REPORT

7. Oral Report

J. Fasana discussed Metro’s recent study, “Understanding How Women Travel” which confirmed that women have different mobility needs, travel patterns and commute demands, and some women feel unsafe using Metro for a variety of reasons. He also mentioned the completion of the LA River bike paths.

UPDATE ITEMS

8. Metrolink Update

There was no update given on this item.

9. Gold Line Update

There was an announcement for the signing of the contract on October 4, 2019 with the selected contractor and all are welcome to be present.

EXECUTIVE DIRECTOR'S REPORT

10. Oral Report

There was no update given on this item.

COMMITTEE MEMBER ITEMS

There was no update given on this item.

ANNOUNCEMENTS

There was no update given on this item.

ADJOURN

The meeting was adjourned at 3:57 p.m.

REPORT

DATE: October 24, 2019
TO: SGVCOG Transportation Committee
FROM: Marisa Creter, Executive Director
RE: **SB 743 IMPLEMENTATION PRESENTATION**

RECOMMENDED ACTION

For information only.

BACKGROUND

Governor Brown signed SB 743 (Steinberg, 2013), which creates a procedure to change the way that transportation impacts are analyzed under CEQA. SB 743 changes the focus of transportation impact analysis in CEQA from measuring impacts to drivers, to measuring the impact of driving. The change replaces Level of Service (LOS) with vehicle miles of travel (VMT) and provides an efficient review of land use and transportation projects that will help reduce future VMT growth. This change is expected to better align transportation impact analysis and mitigation results with the State's goals to reduce greenhouse gas emissions, improve public health, and encourage infill development.

Local jurisdictions must determine their VMT baseline for comparison. They can use the baselines found within the California Statewide Travel Demand Model, or they can identify their own baselines. For example, local jurisdictions could also accomplish this through the development of a Climate Action Plan (CAP) that identifies GHG and VMT reduction targets for the jurisdiction and takes into account community-specific targets that are tailored to the local conditions. Once a CAP is developed, jurisdictions can develop multimodal transportation plans that meet those targets. Local jurisdictions must also then adopt thresholds. The Governor's Office of Planning and Research (OPR) recommends a threshold of 15% below the VMT per capita of the surrounding region and/or city. This threshold is consistent with both the Caltrans statewide target for VMT reductions and the urban regional targets for GHG emissions reductions.

Iteris Inc., a global leader in applied informatics for transportation and agriculture, will present an overview of the background to SB 743 and identify the steps required for local agencies to implement the new guidelines by July 1, 2020. The following topics will also be covered in the presentation:

- Rationale behind the change from LOS to VMT
- Potential methodologies for calculating VMT
- Development of screening criteria
- Setting of appropriate VMT impact thresholds
- Identification of potential mitigation measures
- Stakeholder involvement
- Potential implementation challenges and lessons learned

Prepared by:



Navneet Kaur
Management Analyst

REPORT

Approved by: Marisa Creter
Marisa Creter
Executive Director

DATE: October 24, 2019

TO: Transportation Committee

FROM: Marisa Creter, Executive Director

RE: I-605 CORRIDOR AESTHETICS MASTER PLAN

RECOMMENDED ACTIONS

Recommend that the Governing Board approve the I-605 Corridor Aesthetics Master Plan.

SUMMARY

With the development of several I-605 freeway projects, (I-605 Corridor Improvement Project), a need arose to develop a Corridor Aesthetic Master Plan (CAMP) that could be applied to all on-going freeway projects funded by the Measures R and Measure M sales tax measures to include:

- Capacity and operational enhancements
- Additional general purpose lanes
- Ramp reconfigurations
- Toll lanes
- Increased signage

As a need to have a uniform approach to future highway improvements, the Gateway Cities COG formed an Aesthetics Committee which was composed of the Los Angeles County Transportation Authority (LA Metro), the municipal members from the Gateway Cities Council of Governments (GCCOG), the San Gabriel Valley Council of Governments (SGVCOG), and the California Department of Transportation (Caltrans).

The Aesthetics Committee provided background information and community preferences for the overall design. The committee members worked in collaboration to develop a CAMP for the I-605 Project, and as a Committee approved the attached final document.

Each participating agency is now being requested to approve the CAMP with the understanding that these guidelines will be used on any future project constructed along the 605 corridor from the 405 to the 210. For the SGVCOG this means that any project from the 60 to the 210 will need to be in conformance with this adopted document.

BACKGROUND

The I-605 freeway has designated funding under both Measure R and Measure M to address deficiencies, in particular interchanges at the 5 and the 60 as well as selected on and off ramps. A conceptual study of alternatives was completed in 2018 and an Environmental Impact Report is currently being prepared to evaluate the alternatives and to select a preferred alternative. Under


consideration on new freeway to freeway ramps, additional general travel lanes, and potential toll lanes. Many of these improvements would result in new retaining walls, bridge structures, ramps, soundwalls, lights and signage, and landscaping. In order to provide direction on the visual appearance of these improvements and to try to establish a corridor wide approach, it was agreed that a corridor aesthetics plan should be developed and adopted concurrently with the EIR approval. Metro procured a consultant contract to prepare this document and since most of this corridor is within their jurisdiction, the Gateway Council of Governments took the lead in creating an Aesthetics Technical Advisory Committee. SGVCOG staff were included as a member of this Aesthetics Technical Advisory Committee and participated in the monthly meetings and reviewed and commented on the various draft reports. The SGVCOG worked closely with its member agencies that adjoined the 605 corridor and would have an interest in the Aesthetics Master Plan. These agencies included El Monte, South El Monte, Industry, Baldwin Park, Irwindale, and the County of Los Angeles.


The I-605 Corridor Aesthetics Master Plan has now been completed and participating agencies have been asked to formally approve the plan. SGVCOG staff have reached out to our member agencies identified above and have received no objections to the SGVCOG approving this I-605 Corridor Aesthetics Master Plan.

BUDGET IMPACT

This action will have no budget impacts to the SGVCOG. Any future projects that incorporate the aesthetic treatments identified in the plan will include those costs as part of the construction funding for those projects.

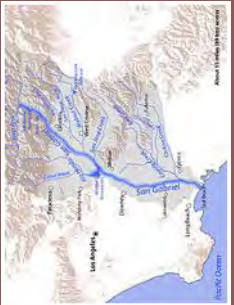
The I-605 CAMP is a general blueprint and it does not prohibit different types of community identification and/or gateway monumentation that may be desired by cities on bridges or retaining walls within their jurisdiction. Generally, these types of additions to the standard freeway aesthetics are considered betterments and funded and maintained by the cities requesting them. During the project design phase, if a city desires these additional treatments, they will need to enter into a separate agreement with Caltrans and fund the associated costs.

Prepared by: 
Mark Christoffels
Chief Engineer

Approved by: 
Marisa Creter
Executive Director

ATTACHMENTS

Attachment A: I-605 Corridor Aesthetics Master Plan



I - 605 CORRIDOR AESTHETIC MASTER PLAN



SAN GABRIEL RIVER FREEWAY



I - 605 CORRIDOR AESTHETIC MASTER PLAN

June 6, 2019

Aesthetic Committee Members

Municipalities

City of Artesia	City of Lakewood
City of Baldwin Park	City of Long Beach
City of Bellflower	City of Lynwood
City of Cerritos	City of Norwalk
City of Compton	City of Pico Rivera
City of Downey	City of Santa Fe Springs
City of El Monte	City of South El Monte
City of Hawaiian Gardens	City of Whittier
City of Industry	Los Angeles County

Gateway Cities, Council of Governments

Kekoa Anderson

San Gabriel Valley, Council of Governments

Mark Christoffels

Los Angeles Transportation Authority (LA Metro)

Ernesto Chavez, LA Metro Lucy Olmos, LA Metro

Isidro Panuco, LA Metro Catherine Saint, LA Metro

California Department of Transportation (Caltrans)

Mehdi Salehimik, District 07 Quincy Wong, Bridge Architecture & Aesthetics

George Olguin, District 07 Isaac Tasabia, Bridge Architecture & Aesthetics

Glen Levstik, District 07

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1.0 INTRODUCTION

This project is titled the *San Gabriel River Freeway (I-605) Corridor Aesthetics Masterplan* (hereafter referred to as the “I-605 CMP”).

1.1 Interstate 605 Aesthetics Committee and Approval Process

With the development of several I-605 freeway projects, (I-605 Corridor Improvement Project), a need arose to develop a Corridor Aesthetic Master Plan (CMP) that could be applied to all on-going freeway projects funded by the Measures R and Measure M sales tax measures to include:

- Capacity and operational enhancements
- Additional general purpose lanes
- Ramp reconfigurations
- Arterial intersection enhancements
- Increased signage
- Safety features

As a need to have a uniform approach to future highway improvements, the Gateway Cities COG formed an Aesthetics Committee which is composed of the Los Angeles County Transportation Authority (LA Metro), the municipal members from the Gateway Cities Council of Governments (GCCOG), the San Gabriel Valley Council of Governments (SGVCOG), and the California Department of Transportation (Caltrans).

The Aesthetics Committee provided background information and community preferences for the overall design. The committee members worked in collaboration to develop a CMP for the I-605 Project, and will provide aesthetics review, oversight, and approval as the design is refined in the design-build process.

The I-605 CMP serves to plan the visual quality of the I-605 transportation corridor. Therefore, all future transportation projects along the corridor shall have an accompanying document which identifies the appearance of planned physical elements, to include landscape and structures; which when fully implemented, shall reflect the committee’s overall vision for the I-605 corridor. The Design-Build Contractor (“Contractor”) will work closely with the I-605 Aesthetics Committee to ensure adherence to the I-605 CMP.

1.2 Context Sensitive Solutions

The I-605 CMP incorporates the Caltrans policy of “Context Sensitive Design Solutions” (CSS) which is “an approach to plan, design, construct, maintain, and operate its transportation system. These solutions use innovative and inclusive approaches that integrate and balance community, aesthetic, historic, and environmental values with transportation safety, maintenance, and performance goals. CSS are reached through a collaborative, interdisciplinary approach involving all stakeholders.” (Director’s Policy DP-22, Context Sensitive Solutions)

The purpose of CSS is to develop highway design elements and structures that are responsive to local values and concerns, through involvement of stakeholders, early in the planning stage. These interventions provide consistency and unity in the appearance and image of the route, through application of corridor aesthetic themes; while enhancing individual community identities. For the I-605 CMP, emphasis was placed upon the history of the region, local geography, and the existing freeway structures and communities to produce the “Aesthetic Committee Master Plan Theme”. The Aesthetics Committee combined the diverse features into a comprehensive design that represents the totality of the I-605 corridor.



1.2.1 History of the Region - Historical & Cultural Considerations

The San Gabriel Valley is abundant with history of its early settlers and civilizations. From the aboriginal Tongva people, to the arrival of Spanish explorers and establishment of the Mission San Gabriel Arcangel. The Spanish (and later Mexican governments) also encouraged settlement by issuing large land grants called *ranchos*. Typically, the Ranchos encompassed the most valuable land near the Pacific coast, and inland near major rivers, which developed the foundation for California’s agricultural heritage and land surveying system.

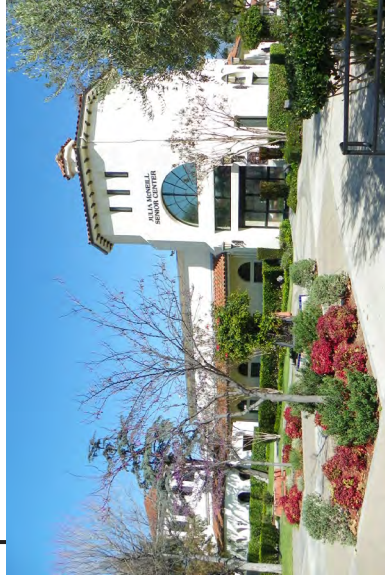
These historic ranchos of early California were a primary influence on the “Aesthetic Committee” Master Plan Theme, and are reflected in many of the design elements.

One most notable rancho is Rancho Los Nietos. It was one of the first and largest land grants. The rancho was so vast, that today, the following communities

were located within its border: Anaheim, Artesia, Buena Park, Bolsa Chica, Cerritos, Cypress, Downey, Fullerton, Garden Grove, Huntington Beach, Lakewood, Long Beach, Los Alamitos, Naples, Norwalk, Santa Fe Springs, Seal Beach, Sunset Beach, and Whittier (*Wikipedia, Jan. 2019*).



Land development in the 20th and 21st century often followed the boundaries of the ranchos, and often retain the original name. Many of the cities continue to embrace this rancho theme through preservation and restoration of municipal buildings and street aesthetics.



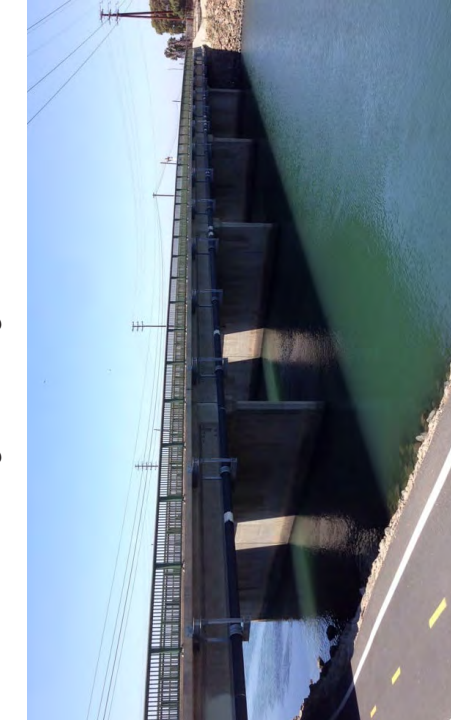
1.0 INTRODUCTION

1.2.2 Local Geography - Environmental Considerations

Another primary influence on the “Aesthetic Committee Master Plan Theme” is the San Gabriel River. The San Gabriel Valley, situated between the San Gabriel Mountains to the North, and the Pacific Ocean to the South, derives its name from the San Gabriel River which runs parallel to the San Gabriel Freeway. Much of San Gabriel Valley’s histo-



ry originates with the river. From the early Tongva people relying on its waters for fish and surrounding game, to the Spanish and Mexican settlers using it for irrigation and ranching. Today, the San Gabriel River is still a major source of water for the local communities, and provides several recreation areas from the upper forks in the San Gabriel mountains, to the wetland areas near the Pacific. The significance of the river is reflected in the retaining wall design motif. The San



Gabriel River flows through the San Gabriel Valley carrying its history and connecting the communities to their shared heritage.

1.2.3 Existing Freeway Structures and Communities

Interstate 605, officially known as the San Gabriel Freeway, is a major Interstate Highway in the Greater Los Angeles area running approximately 27 miles from Duarte to Seal Beach.

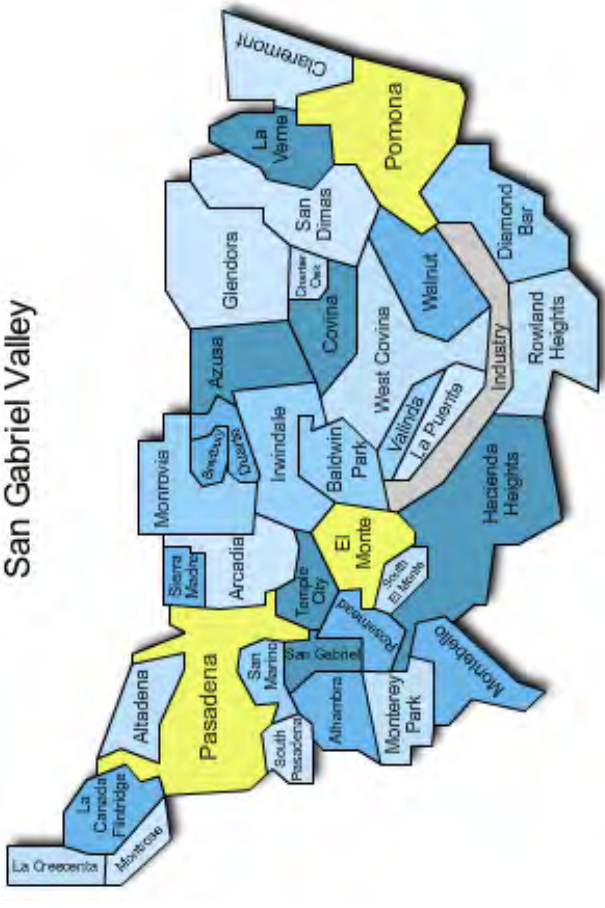
Development of the I-605 Corridor theme was sensitive to the visual context of existing I-605 structures that will remain. Expressions of the “Aesthetic Committee Master Plan Theme” will be uniformly applied to the I-605 Project. Special consideration was directed at incorporating, or retrofitting existing structures with new aesthetic treatments to provide a unified aesthetic appearance. Examples will be matching the overall geometries of new and older structural components (columns etc.) at widened bridge locations.



I-605 aesthetics will blend influences of Interstate 5 where the highways cross. I-605 bridge structures which cross over the I-5 Corridor, will incorporate the same barrier, box girder, column, and abutment/slope paving as found on I-5 structures to maintain visual unity. Other design influences include the I-5 sound wall pilaster caps and sound wall block textures.

The aesthetics of visible structures will impact surrounding neighborhoods and communities along the I-605 Corridor. Most of the structures primarily seen are retaining walls, sound walls, and undercrossings. Their scale, shape, proportions, textures, color and materials not only need to reflect the corridor’s theme, but also integrate with adjacent community elements. Highly visible I-605 undercrossings will have opportunities for aesthetic enhancements such as; pilasters, gateway monuments, community identifiers, etc. to enhance the identity of each community.

San Gabriel Valley



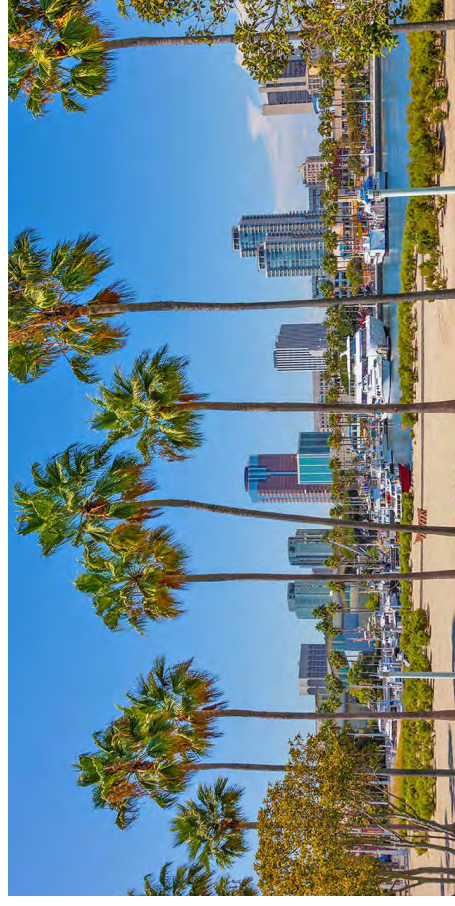
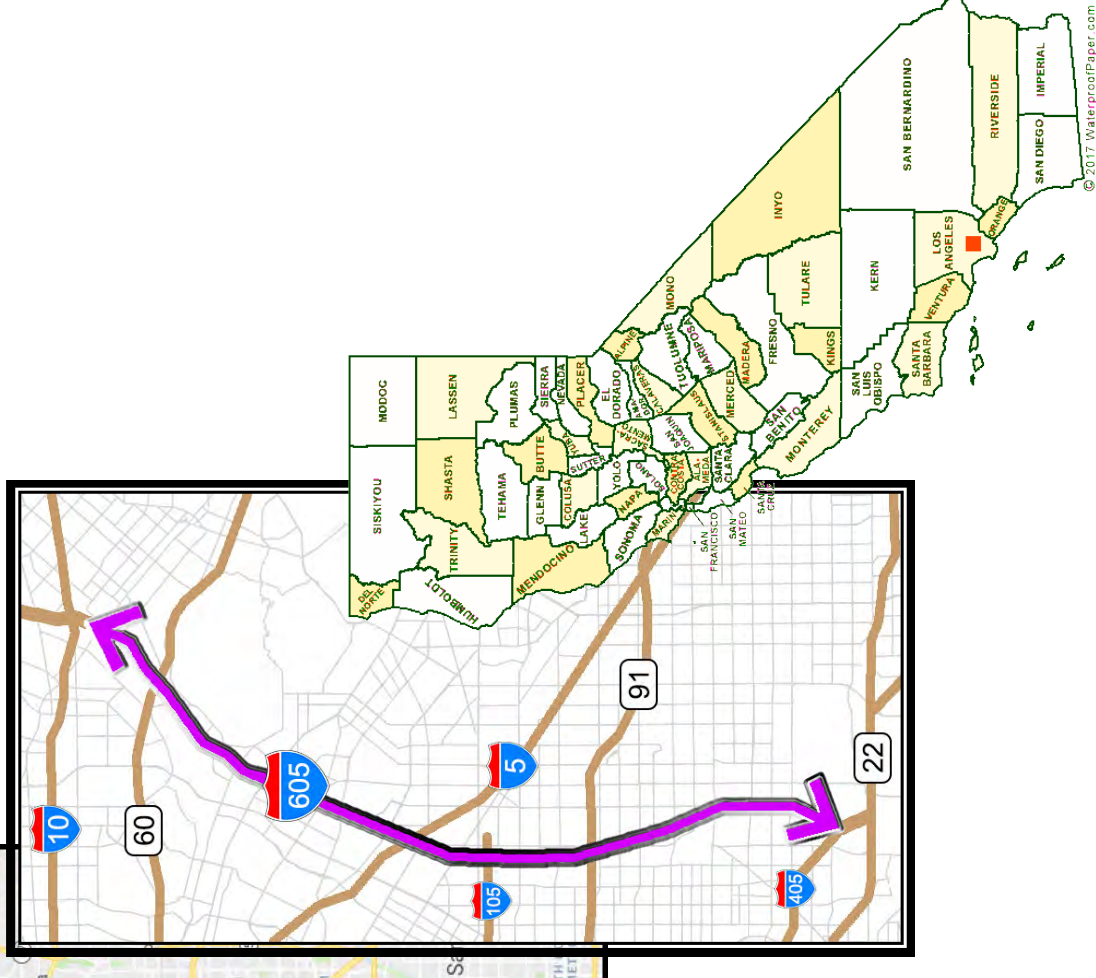
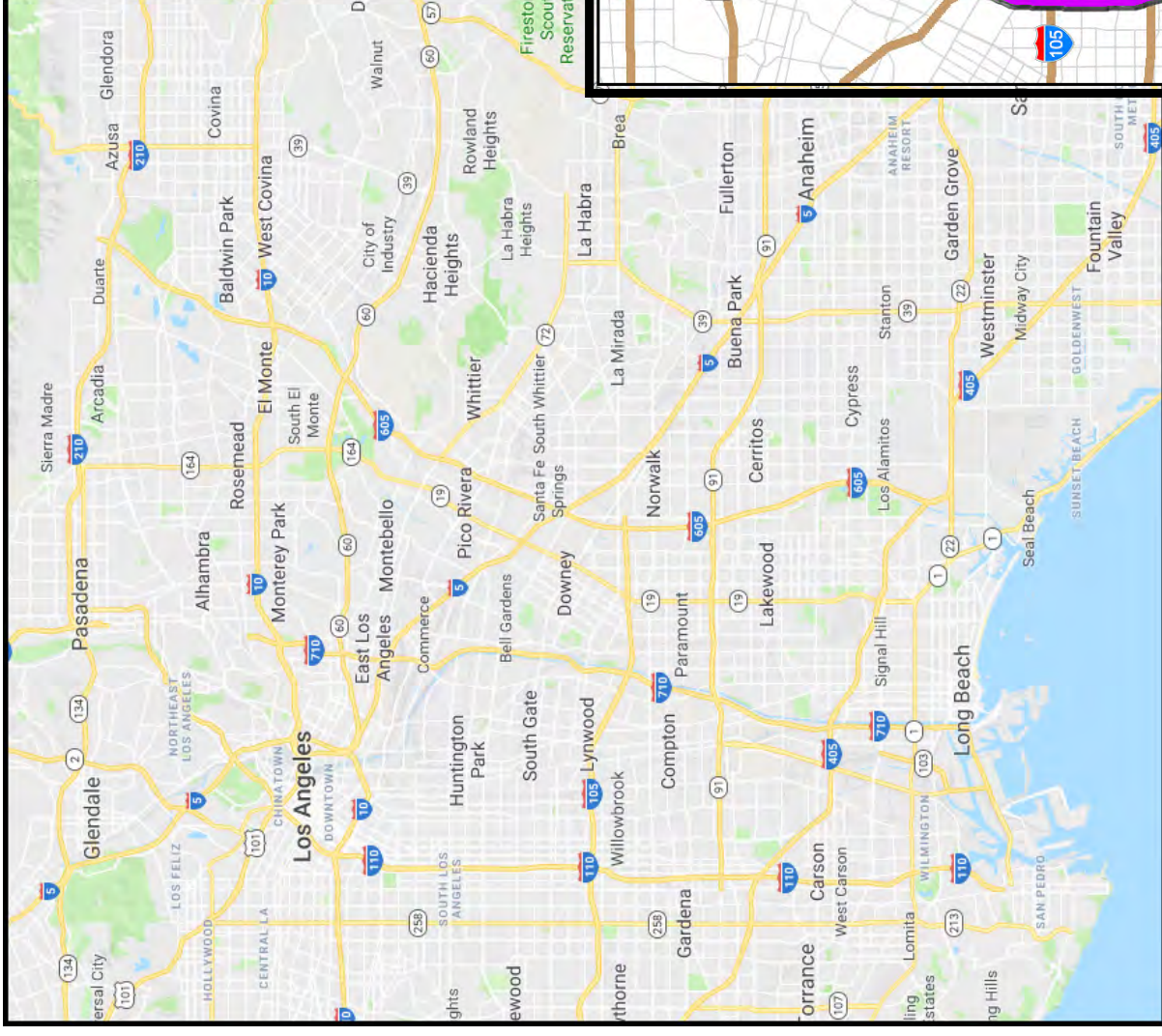
2.0 SITE ANALYSIS

2.1 Project Site

The San Gabriel Valley, approximately 280 square miles in size, is one of the principal valleys of Southern California. It is located primarily to the east of the city of Los Angeles, with a population of approximately 1.5 million people and includes 47 neighborhoods.

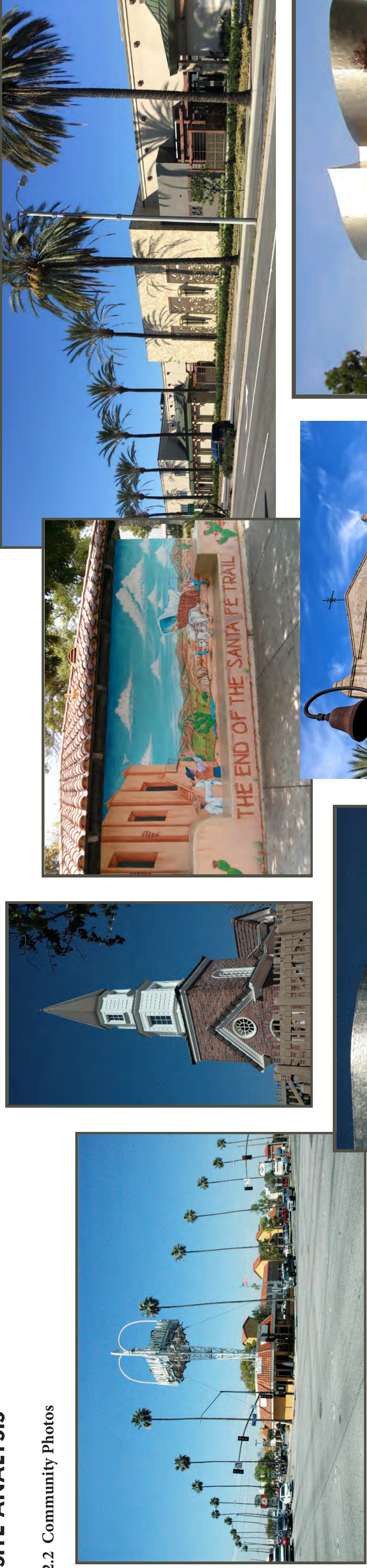
The I-605 Corridor is a pillar of the Southern California freeway system, connecting commuters, travelers, and truckers moving between the densely populated Gateway Cities, San Gabriel Valley, the Inland Empire, and the rest of the region. The I-605, known as the “San Gabriel River Freeway”, is heavily used on a daily basis for local, regional, interregional, and interstate travel and commerce, including thousands of trucks traveling between the Gateway Cities and various points inland for the distribution of goods. The Corridor is home to ethnically diverse and historically rich neighborhoods and supports thriving commercial and industrial centers. *(Courtesy of LA Metro)*

The San Gabriel Valley is home to leading educational and research institutions, such as; the California Institute of Technology (CalTech), California State Polytechnic University, Pomona (Cal Poly), NASA's Jet Propulsion Laboratory, and the City of Hope Medical Center. The San Gabriel Valley is a well-connected hub with seven freeways, two major rail lines, and close proximity to the Port of Long Beach and the Port of Los Angeles.



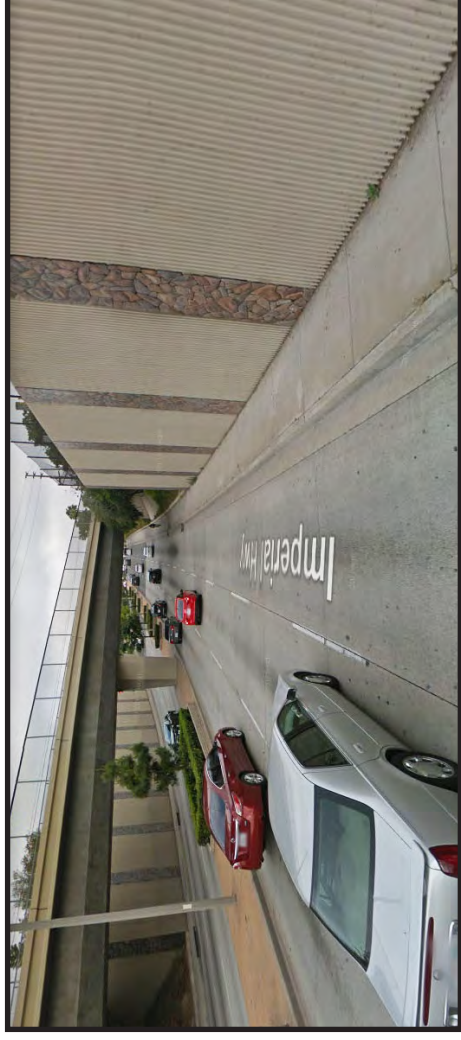
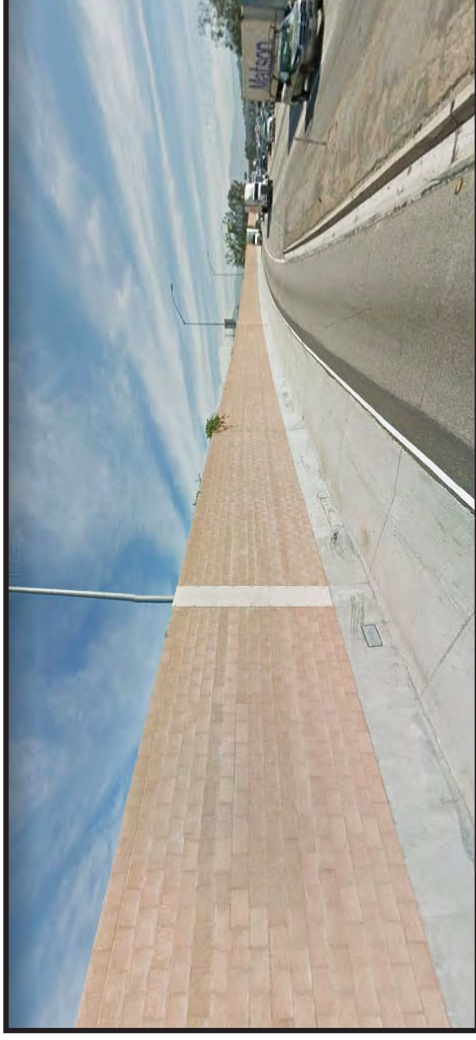
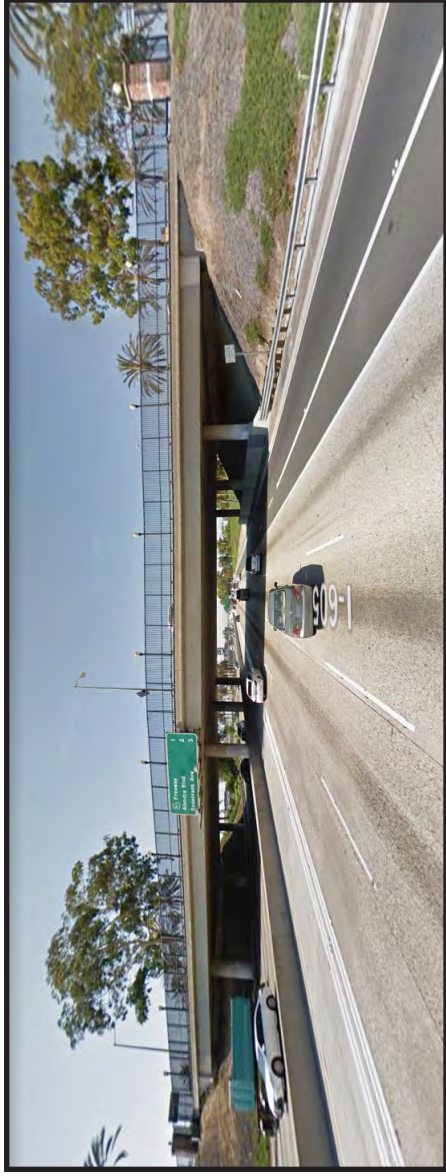
2.0 SITE ANALYSIS

2.2 Community Photos



2.0 SITE ANALYSIS

2.3 Existing I-605 Corridor



3.0 PROJECT AESTHETICS

Aesthetics - The science or study of the qualities of beauty, and a conception of what is artistically pleasing to the senses which may include: light, shadow, line, and color, but not limited to physical forms.

3.1 Goals and Objectives

- **Create an Aesthetically Pleasing Freeway Corridor.**

The I-605 freeway is a major transportation corridor. The freeway is seen and heard by many more local residents as well. It is important to create an aesthetically pleasing corridor for all freeway users and viewers.

- **Ensure Visual Unity Within the Corridor for Motorists.**

All elements viewed predominantly by freeway users should exhibit a single, unified theme and design expression. For example, the overcrossing structures viewed mainly by freeway users should follow the same corridor design theme.

- **Enhance Community Identity.**

In areas more prominent to local travelers, such as under crossings and the community side of sound walls, aesthetic designs responding to local community needs will be allowed. In addition, expressions of community identity may be allowed on sound wall pilasters, abutment walls and other locations compatible with the goals of visual unity and safety.

- **Promote Design for Safety, Functionality, and Maintainability.**

Design for safety is a concept emphasizing the use of durable materials sited for safe access by maintenance personnel. The concept recognizes that all materials must be maintained for proper function and longevity, but smart choices will require less repetitive maintenance over the years. Proper placement of materials will allow personnel to do their work in greater safety.

- **Maintain Budget and Schedule.**

The intent of these guidelines is to give practical direction to future designers. Solutions should not require excessive capital or operational/maintenance outlay nor delay the approved schedule.

3.2 Community Identification and Gateway Monuments

Community Identification is a concept developed by Caltrans to allow surrounding cities to display and express their unique attributes within the right-of-way. This can include visual images, graphics, text, or sculptural artwork. Community Identifiers are placed on

structures such as: bridges, bridge abutments, bridge rails, slope paving, or retaining walls. Gateway monuments are freestanding structures or signs that communicate the name of a local city introducing the traveler to the community.



GATEWAY EXAMPLE

3.3 Transportation Art

Transportation Art is a permit program for pre-existing walls such as sound walls. Art applied to walls as part of the widening project is considered an integral part of the freeway design package. Art added at a later time is subject to the Transportation Art Guidelines and is reviewed through the permit process.



3.4 Funding

Many of the aesthetic treatments recommended in this report are above and beyond Caltrans standards.

As with many highway improvement projects, there will be multiple funding sources necessary to support construction of these treatments. Partial funding for the I-605 Corridor Improvement Project is through Measure R, and Measure M. LA Metro will pursue additional federal, state and other sources for future phases of this project. Additional or specialty treatments above and beyond the aesthetics proposed in this masterplan will be the responsibility of the local jurisdiction.

3.5 Aesthetic Treatment Opportunities

There will be new bridge structures in the corridor. In addition, there are a number of new and replacement retaining wall and sound walls. The "Aesthetic Committee Master Plan Theme" will be applied at all structures such as: bridges, overcrossings, undercrossings, pedestrian overcrossings, viaducts, interchanges, connectors, retaining walls, sound walls, etc. Architectural treatments to these structures will be important opportunities to beautify and enhance visual continuity in the corridor. All new structures shall be integrally colored concrete to closely match Davis Colors color #5237 (Sandstone).

The existing landscaping will be removed by freeway widening. Replacement plantings will be implemented. However, many sections of the widened freeway will be paved from wall to wall or fence to fence because the spaces created will be too narrow to maintain safety. The new planting will be mainly in the interchange areas.

3.6 Guidelines for Structures

This section contains aesthetic treatments that will be applied to major structural elements throughout the I-605 Project.

- Bridges

A beautiful bridge makes a minimal impression on the environment and is harmonious to its location. It possesses sound visual character and maintains good proportions both in its integral and spatial components. It is composed of one dominant structural system using a minimum number of bents with minimal columns per bent.

3.0 PROJECT AESTHETICS

Architectural and aesthetic design principles such as balance, scale, movement, repetition, emphasis and unity are utilized on the superstructure to create a visually attractive bridge which achieves the desired appearance.

- Columns

Columns should appear to support the structure as simply as possible. The proportion of elements is important. The goal is a balance between the apparent mass of the superstructure and the size of the column. Columns that are too thin will look spindly, and columns that are too thick will look too squat.

- Slope Paving

The selection of materials under bridges can be varied. Naturally colored river rock embedded in mortar can be designed to be compatible with the established corridor color palettes, textures, and themes. Alternative slope paving treatments for surfaces facing the community could include artistic relief sculptures, patterns, murals, etc.

- Retaining Walls

Retaining walls within the corridor shall have textured form-lined surfaces; utilizing both typical and customized textures and patterns. The form liner method affords flexibility for innovative design, as well as adding interest to retaining wall surfaces. The elevation of the retaining wall should be continuous and compliment the earth it is retaining. Walls that have extreme height variances should be modified to create smoother transitions. Walls that have sharp angles or “kinks” should be rounded and smoothed to create a more fluid appearance.

- Soundwalls

Viewed from the community side, bridge soundwalls are highly visible and imposing because of their height and alignment with undercrossing streets. These large horizontal walls are attached to the bridge barrier and will be seen by the traveling public at much lower speeds than on the freeway side.

- Concrete Barriers

Concrete barriers will be constructed in the median as well as both sides throughout the majority of the corridor for traveler safety. From the community side, in most of the sections, there will be only a very narrow 2-3 feet strip behind the barrier. It is recommended that the area be planted with low growing shrubs to provide relief from the hardscape. Such plantings outside of state right-of-way require a cooperative agreement stipulation that a local entity will maintain the planting after Caltrans installs it.

- Lighting

When selecting lighting, maintenance and vandalism should be considered. Two types of lighting may be used at overcrossings, either the Caltrans standard, or upgraded decorative lighting. All undercrossings should have recessed lighting above pedestrian walkways for safety and security. Lighting beyond Caltrans standards on overcrossings or undercrossings shall be relinquished to the cities. Therefore, the choice of lighting on bridges throughout the corridor will be dependent on the city with jurisdiction over that area.

- Fencing

Three types of fences are proposed for use in the I-605 corridor: I-605 theme fencing, Caltrans standard chain link, and post and cable. The I-605 theme fencing consists of an arch design, 1-inch grid of vinyl-coated mesh, black painted finish, and will be used at structures and overcrossings. It is also recommended in areas with high visibility to freeway users including areas where the views toward businesses on the frontage road have been preserved.

The theme fence will be located on top of the concrete barrier, and top of curb (as with pedestrian overcrossings). Galvanized chain link fence per Caltrans standard specifications will be used to divide the corridor right-of-way from the local areas in order to prevent pedestrians from entering the freeway right-of-way. Post and cable fencing (per Caltrans standard specifications and OSHA requirements) will be used along the top of retaining walls to enhance the safety of freeway maintenance workers.

- Paving/Hardscape

The color, texture, pattern and material of hardscape at various locations should be coordinated. Rock blanket is the preferred gore paving material for consistency with the newly widened I-5 south of the Orange County line.

Gore areas less than 30 feet wide and narrow areas less than 20 feet wide can be difficult locations for maintenance personnel to work in. These areas are considered hard to maintain. Paving of gore and narrow areas is recommended.

- Utilities

Electrical boxes, monitoring equipment, pump houses and other facilities associated with utilities should be consolidated and their placement integrated with the surrounding landscape.

- Structural Components

Note: Important consideration must be given to the detailing of concrete surface treatment to prevent any conflicts with the component's structural section. This includes barriers, wing walls, abutments, slope paving, exterior girders, retaining walls, and columns. The structural component's design shall maintain specific clearances from the concrete's surface to the reinforcement. See “California Amendments to AASHTO LRFD Bridge Design Specifications” page 5-189A, Table 5.12.3-1 and “Bridge Design Details” Section 1-4.1. The surface treatment must not intrude into this clearance. Also, the details provided are intended as examples only. They are not to be used for specific designs.

NOTE: ALL ARCHITECTURAL DETAILS ARE APPROVED FOR AESTHETIC PURPOSES ONLY.

3.7 Guidelines for Other Elements

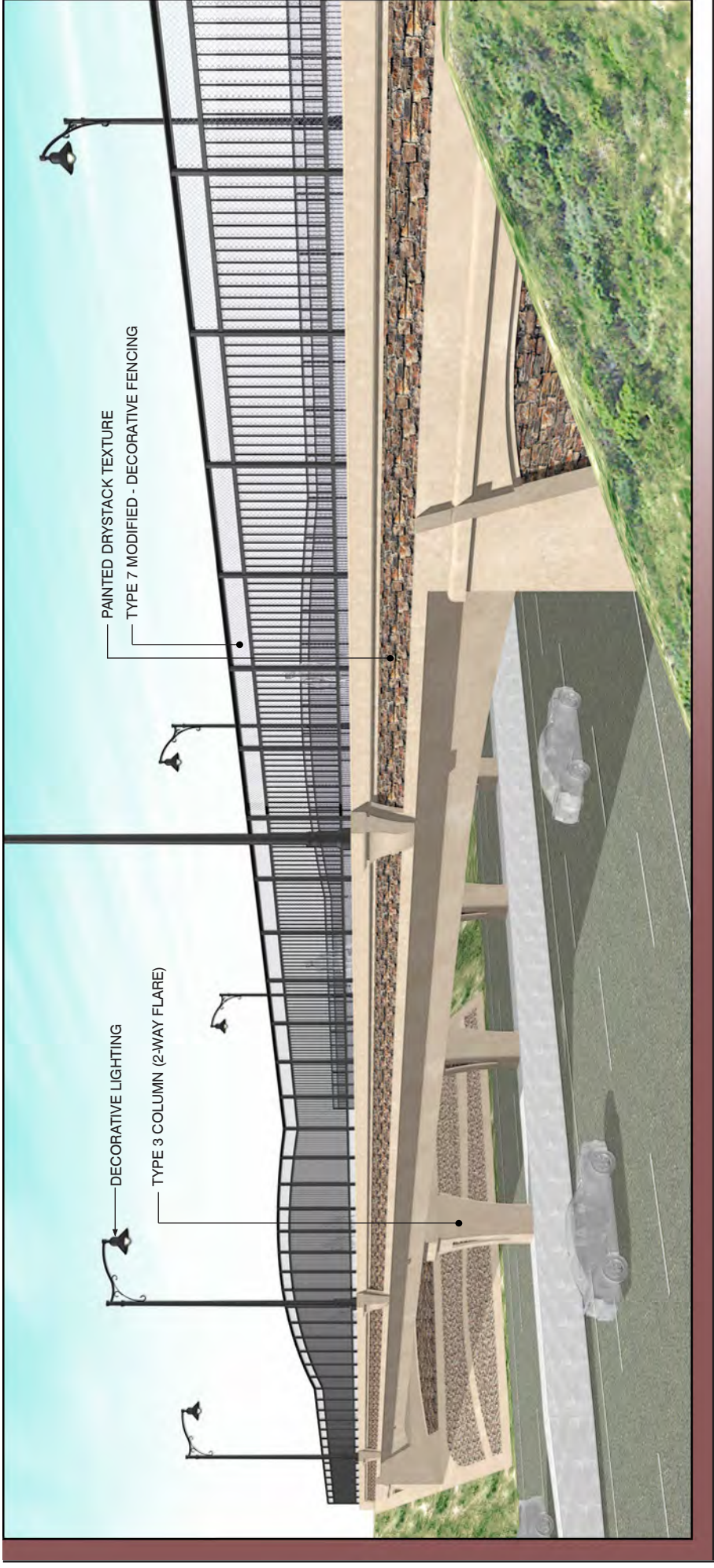
In addition to landscaping and major structural elements of bridges, sound walls and retaining walls, other elements such as undercrossings, fences, paving, lighting, utility cabinets and community art together make an impact on the overall appearance of the freeway corridor. These elements should be coordinated and integrated into the corridor theme. The treatment as well as placement of these elements should be considered and reviewed as early as possible.

4.0 STRUCTURAL AESTHETICS

4.1 OVERCROSSINGS

An overcrossing is a structure carrying a county road or a city street over a state highway, and occurs at various locations along the I-605 Corridor.

Replaced or widened bridges will often be intermixed with existing bridges which have different architectural styles. The overall geometry of replaced or widened structures will be similar to remaining structures to provide visual unity. The “Aesthetic Committee Master Plan Theme” will be applied to geometric forms of girders, columns, and abutment walls which responds to the historic and geographic influences of the region.



PERSPECTIVE VIEW



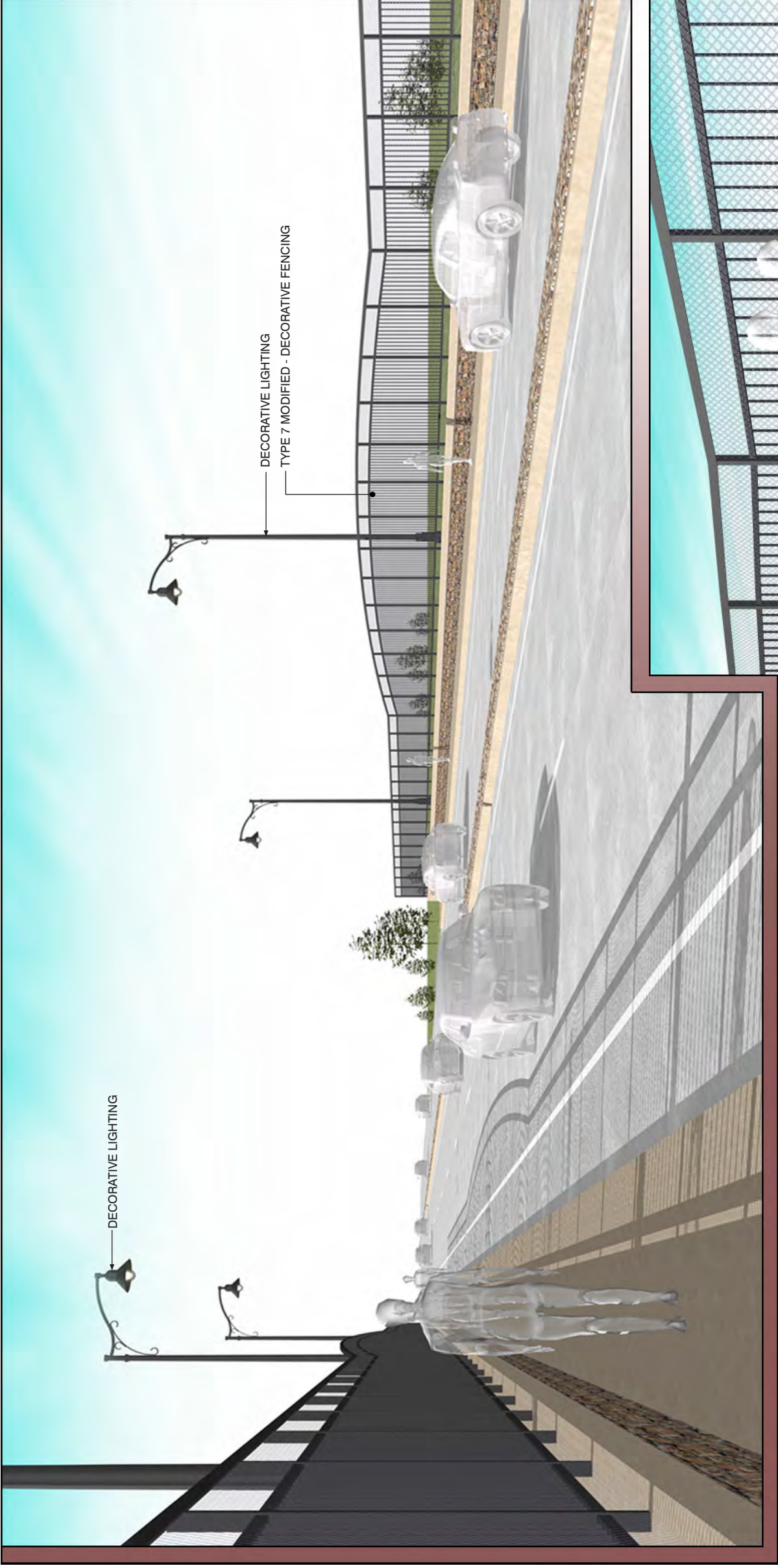
FREEWAY - PERSPECTIVE VIEW

4.1 OVERCROSSINGS



DECORATIVE LIGHTING

TYPE 7 MODIFIED - DECORATIVE FENCING

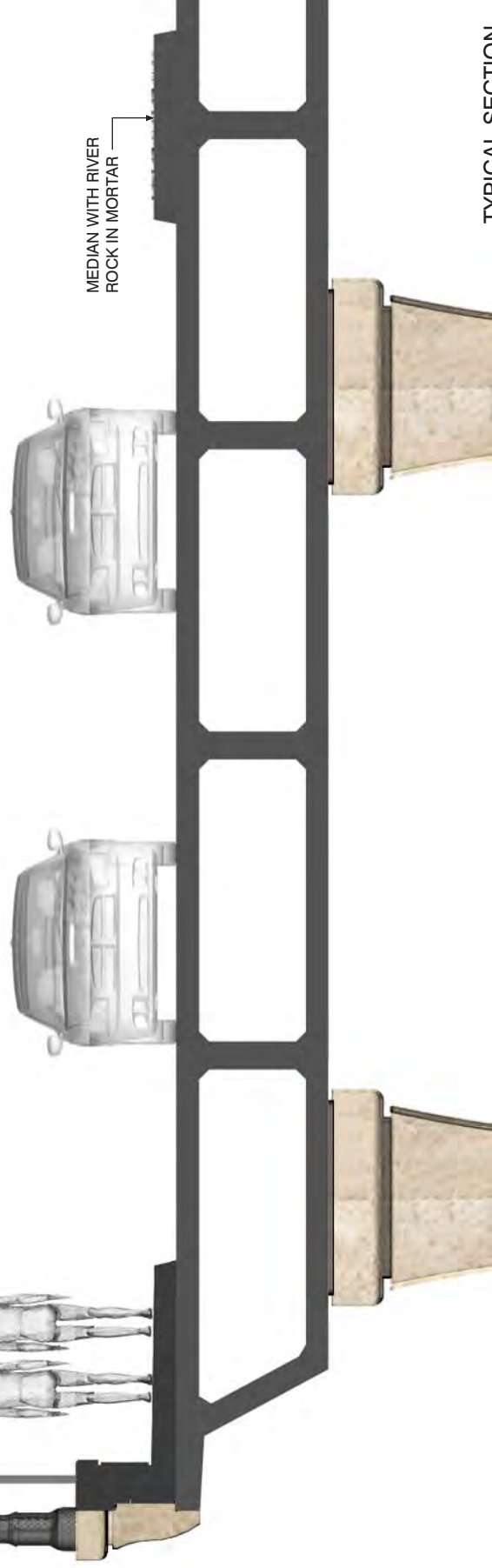


DECORATIVE LIGHTING

DECORATIVE LIGHTING
TYPE 7 MODIFIED - DECORATIVE FENCING

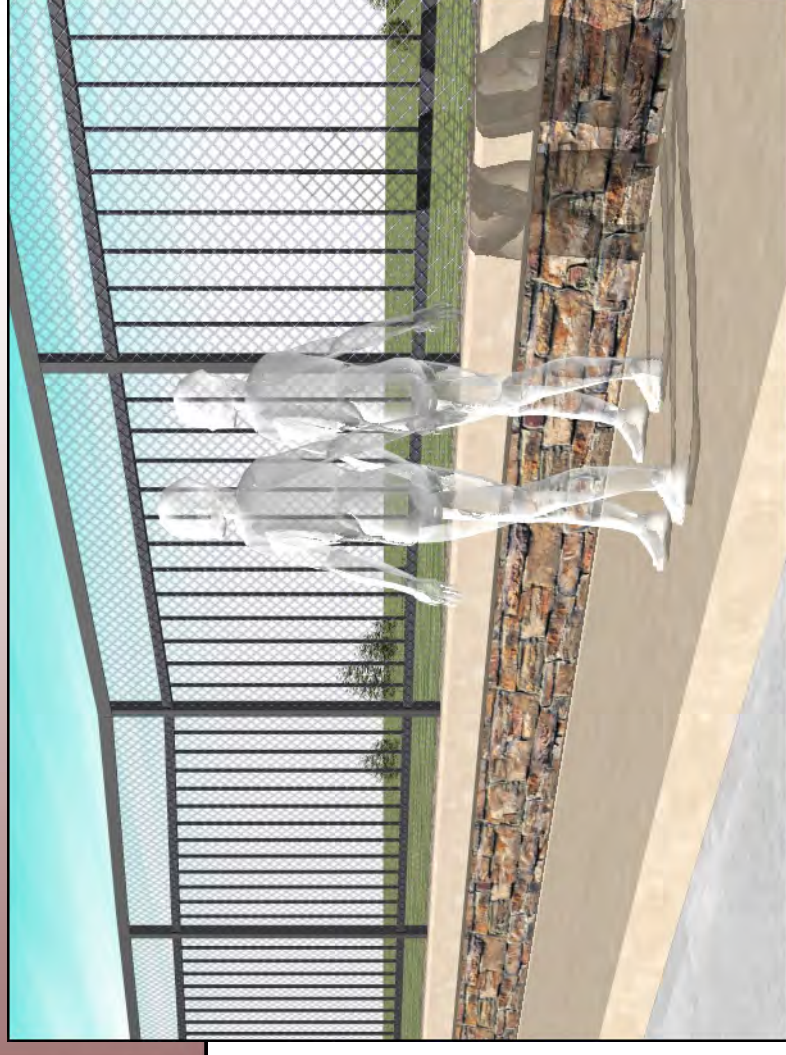


PERSPECTIVE VIEW



MEDIAN WITH RIVER
ROCK IN MORTAR

TYPICAL SECTION



PERSPECTIVE VIEW

FINAL

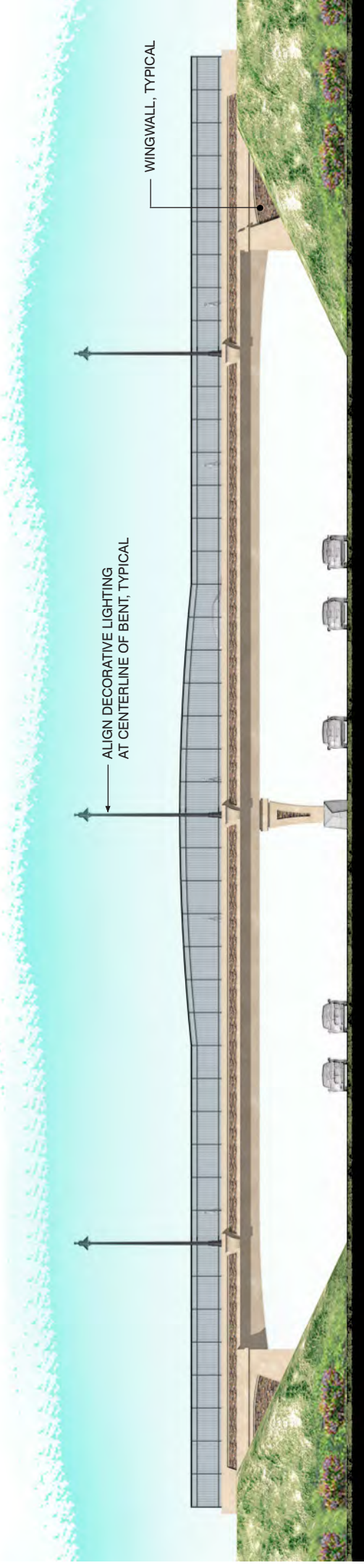
REV 2 - JUNE 13, 2019

I - 605 CORRIDOR AESTHETIC MASTER PLAN

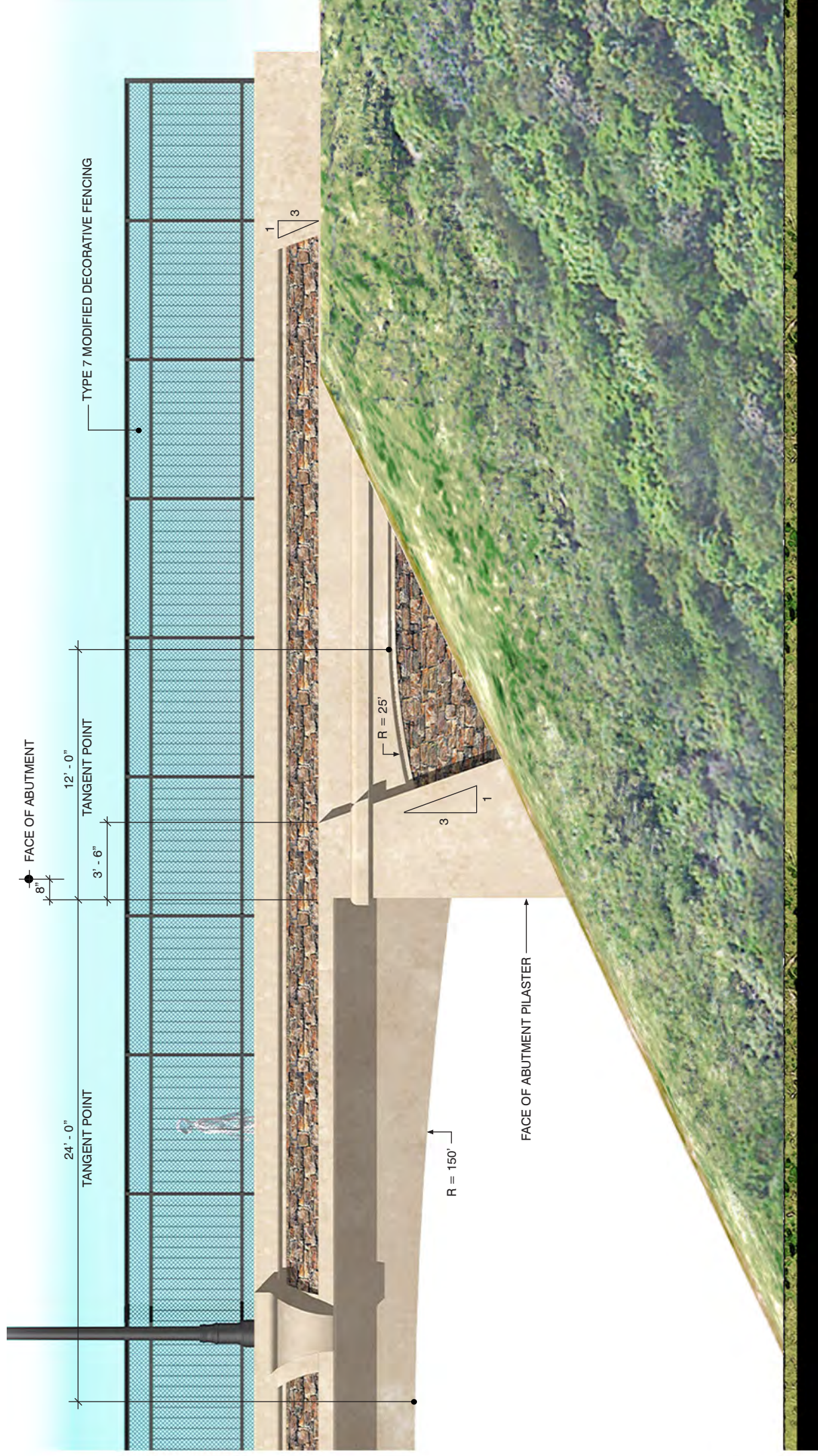
PAGE 12

4.1 OVERCROSSINGS

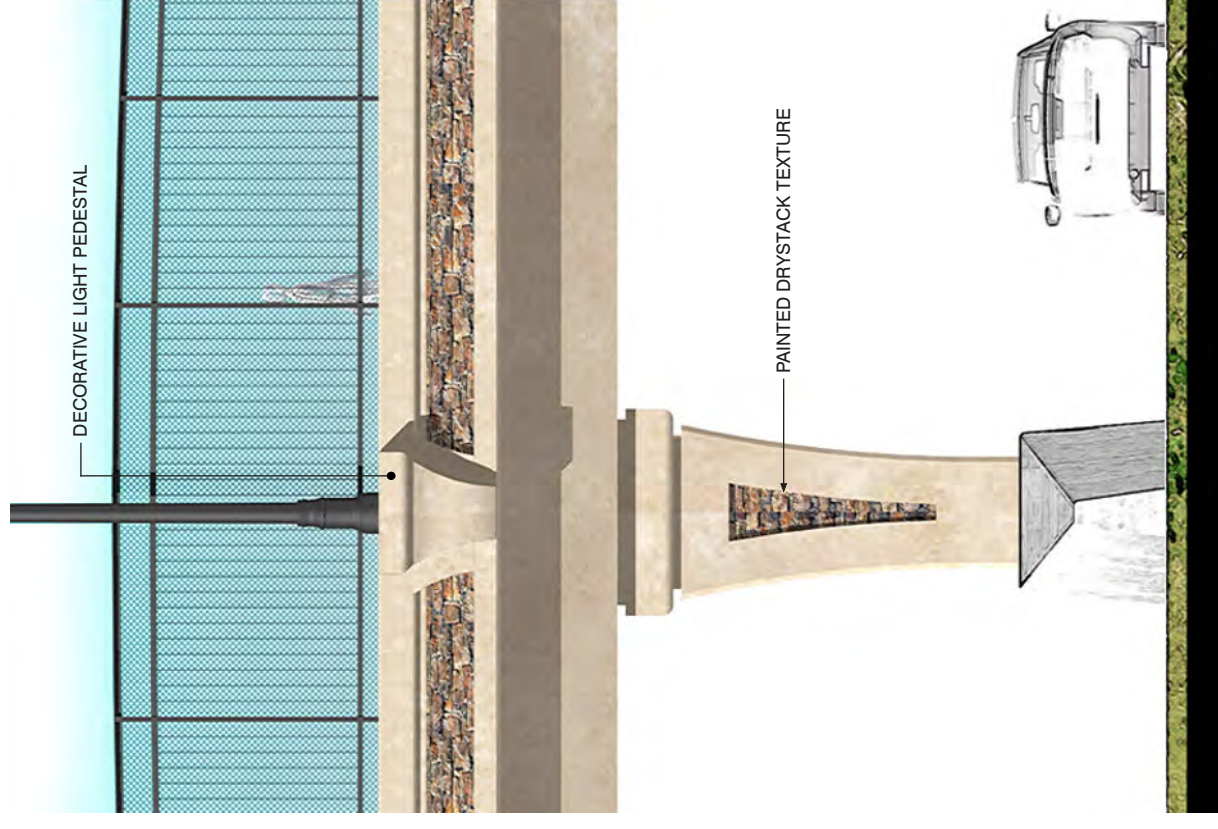
Overcrossings will have haunched girders, 2-way flared columns with textured recess, lighting pedestals, and pilasters at the abutment corners. The architectural treatment consists of integrally colored concrete, painted drystack stone texture, and decorative fencing and lighting with black finish.



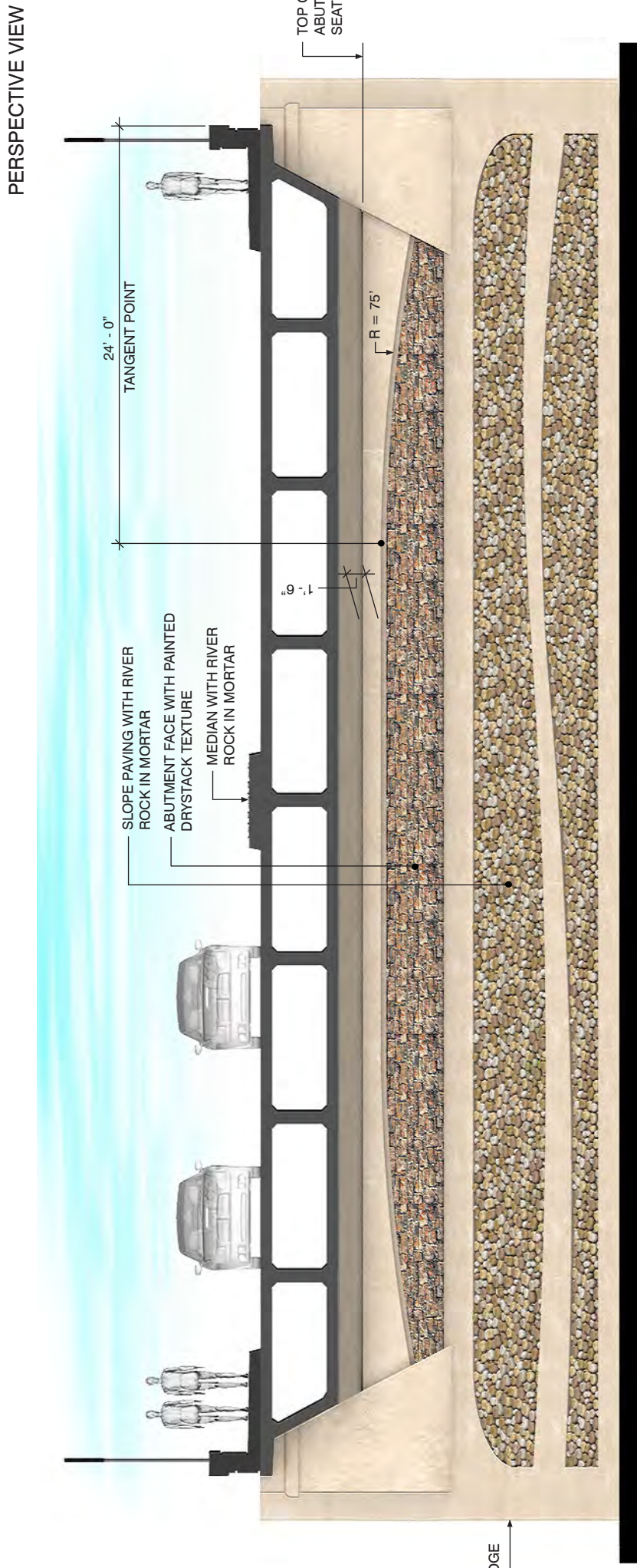
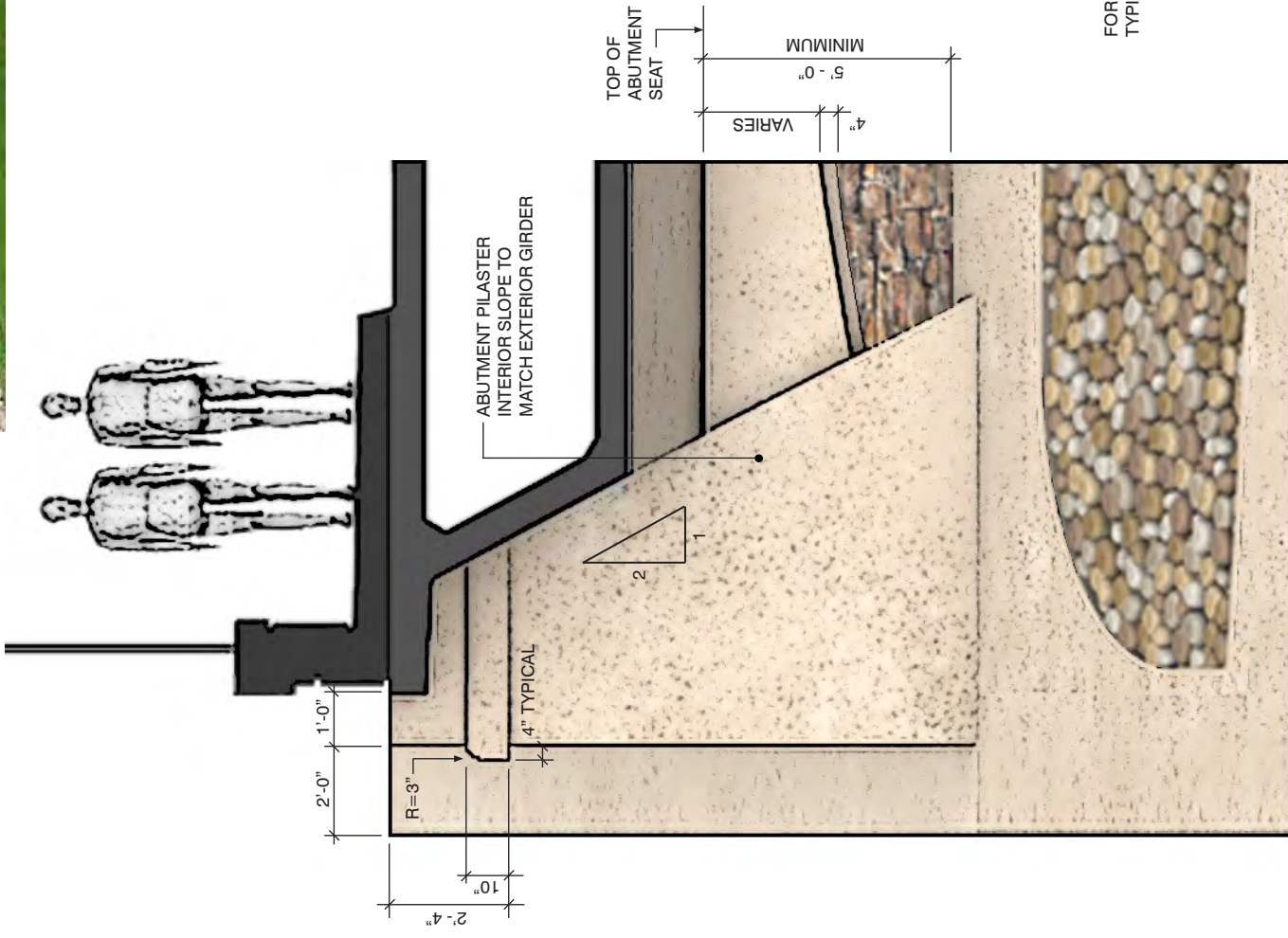
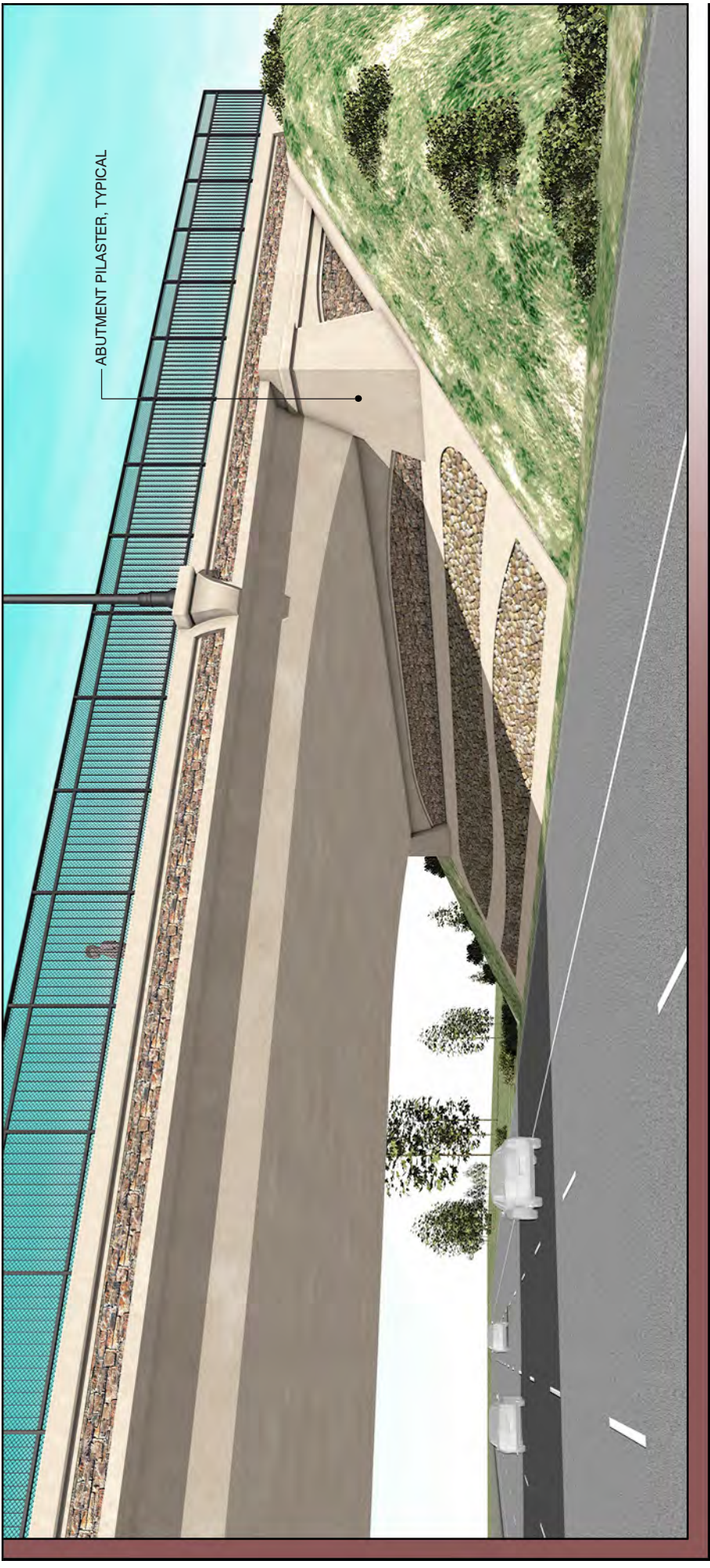
FREEWAY - ELEVATION VIEW



PARTIAL ELEVATION VIEW



4.1 OVERCROSSINGS



FINAL

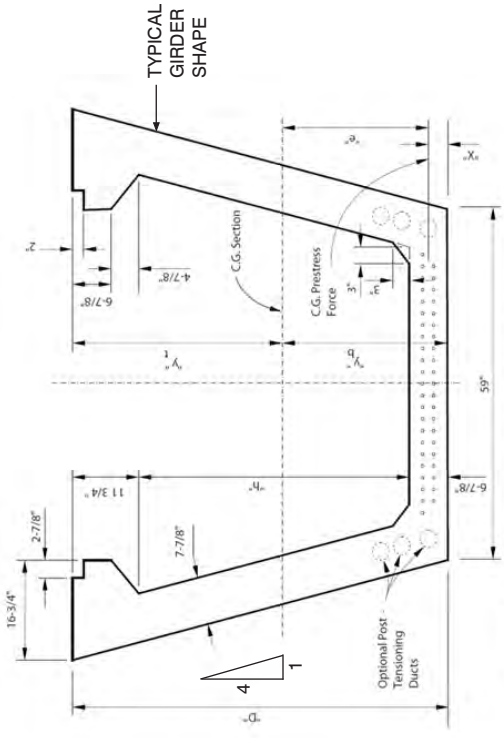
REV 2 - JUNE 13, 2019

I - 605 CORRIDOR AESTHETIC MASTER PLAN

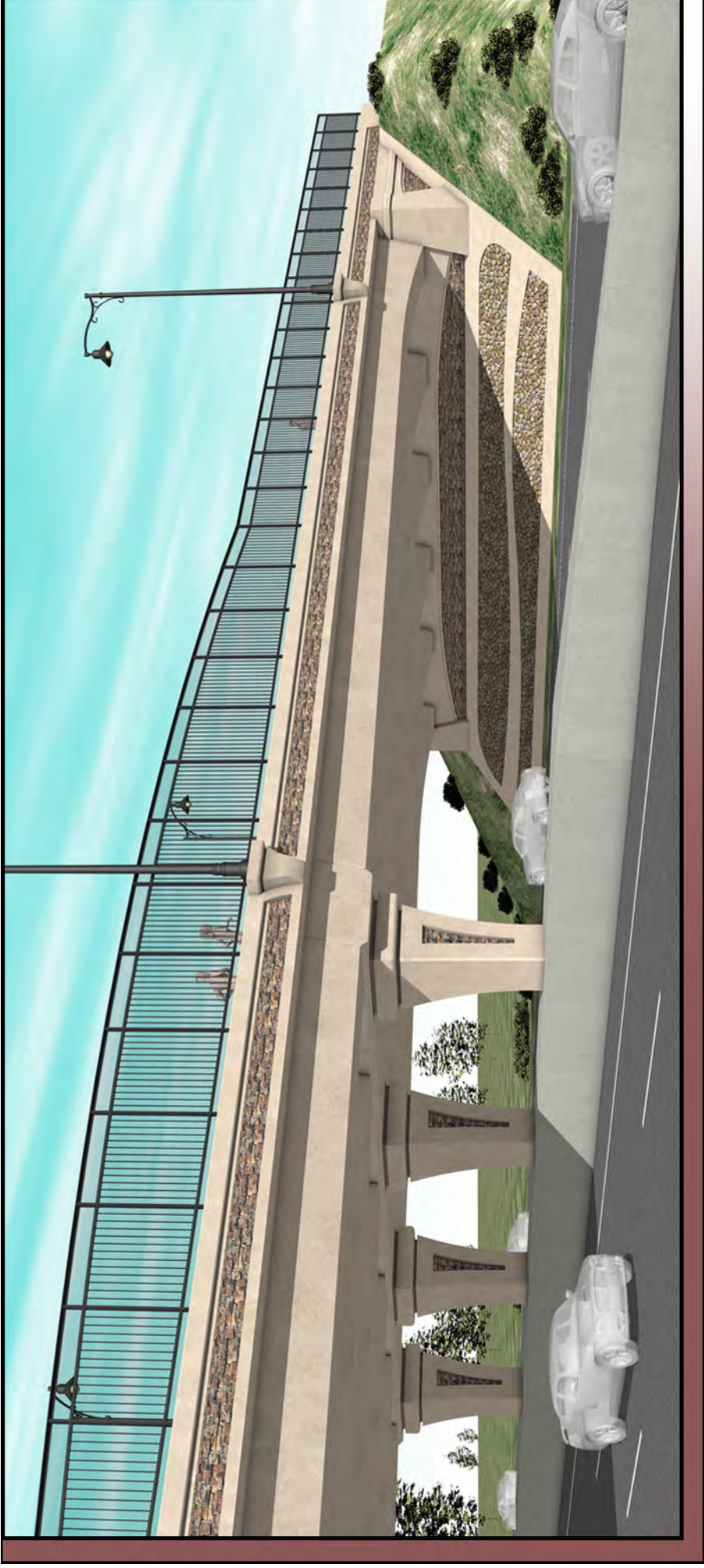
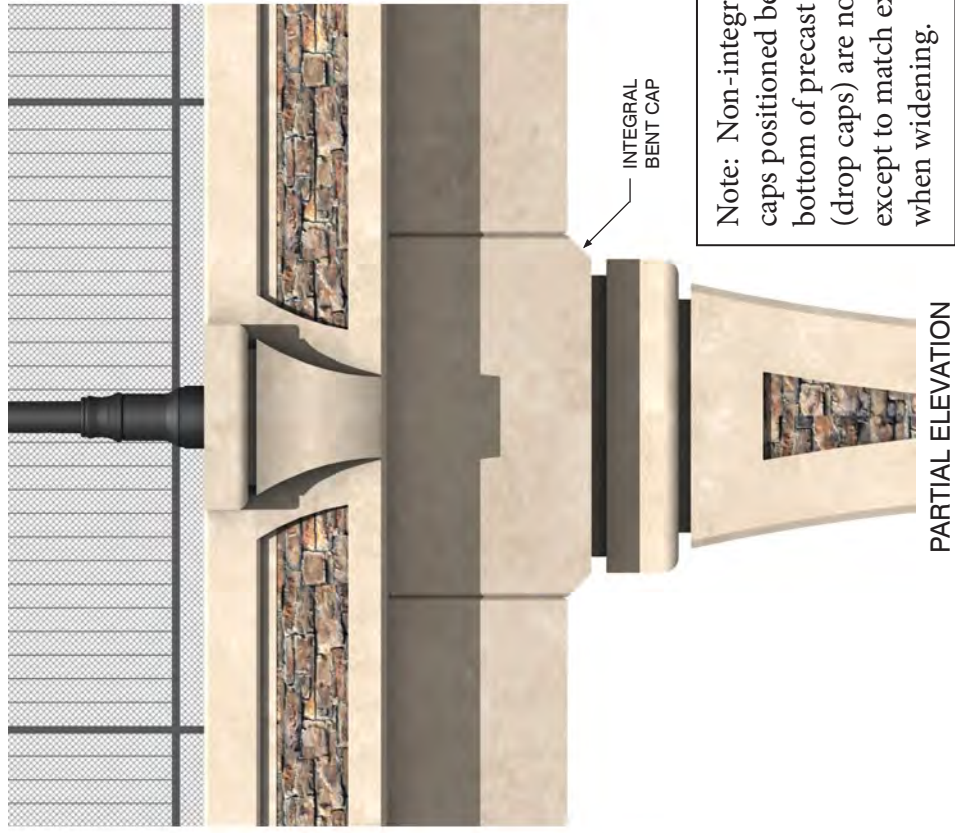
PAGE 14

4.1 OVERCROSSINGS

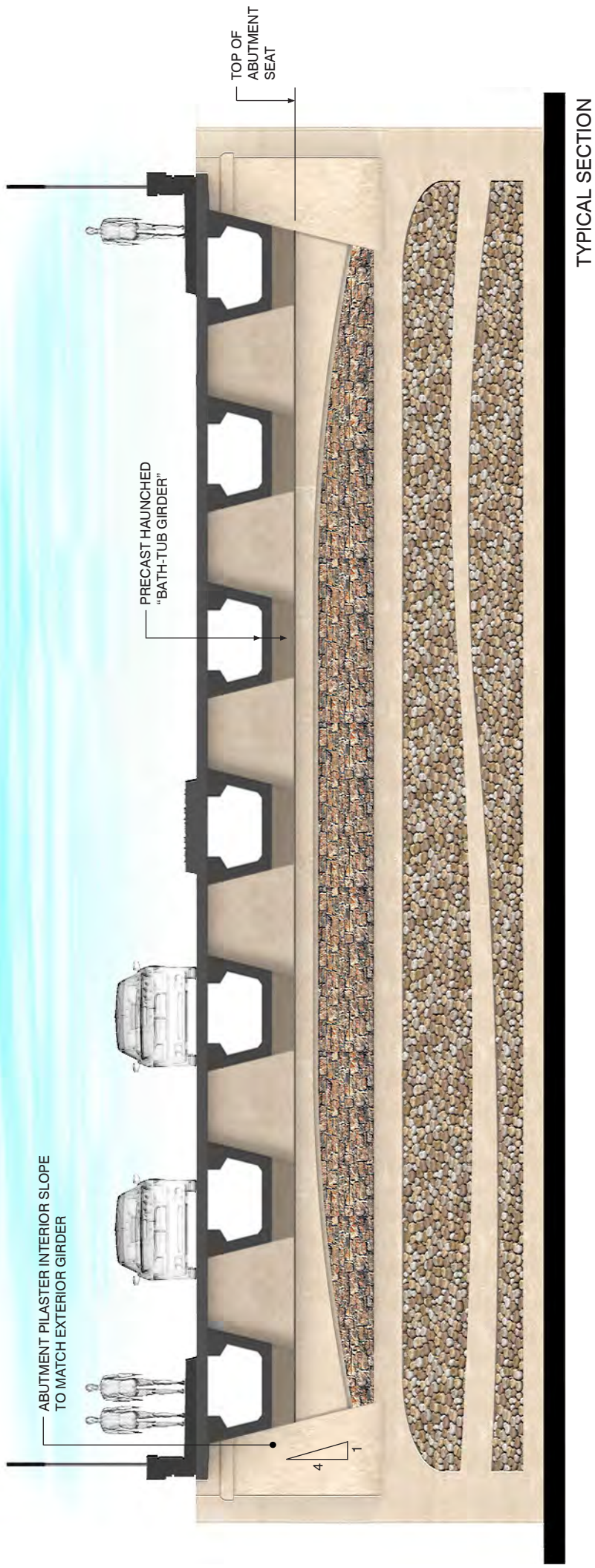
When cast-in-place box girders are not possible, haunched precast bath-tub girders are preferred and should adhere to the "Aesthetic Committee Master Plan Theme".



California Standard "Bath-Tub" Girder



PERSPECTIVE VIEW



FINAL

REV 2 - JUNE 13, 2019

I - 605 CORRIDOR AESTHETIC MASTER PLAN

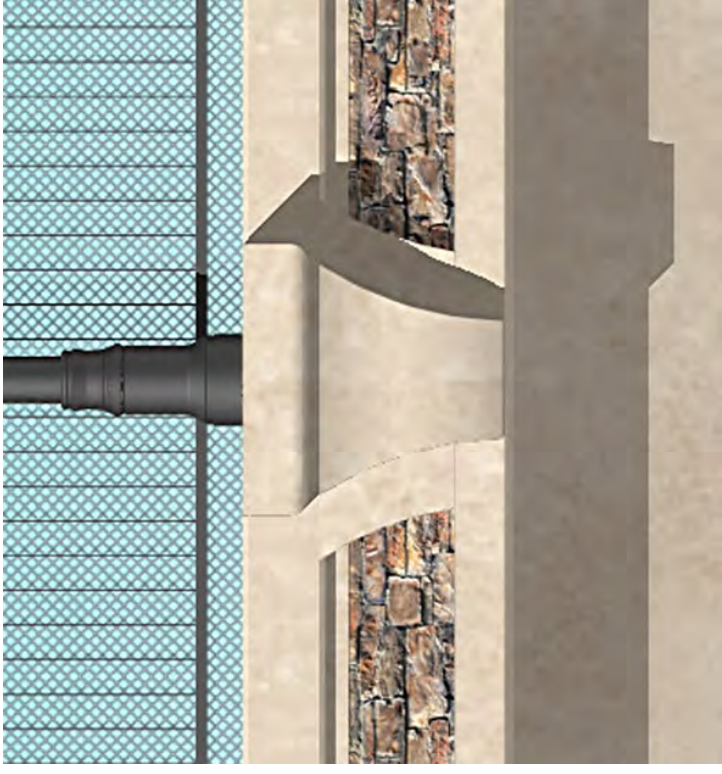
PAGE 15

4.2 BRIDGE DETAILS

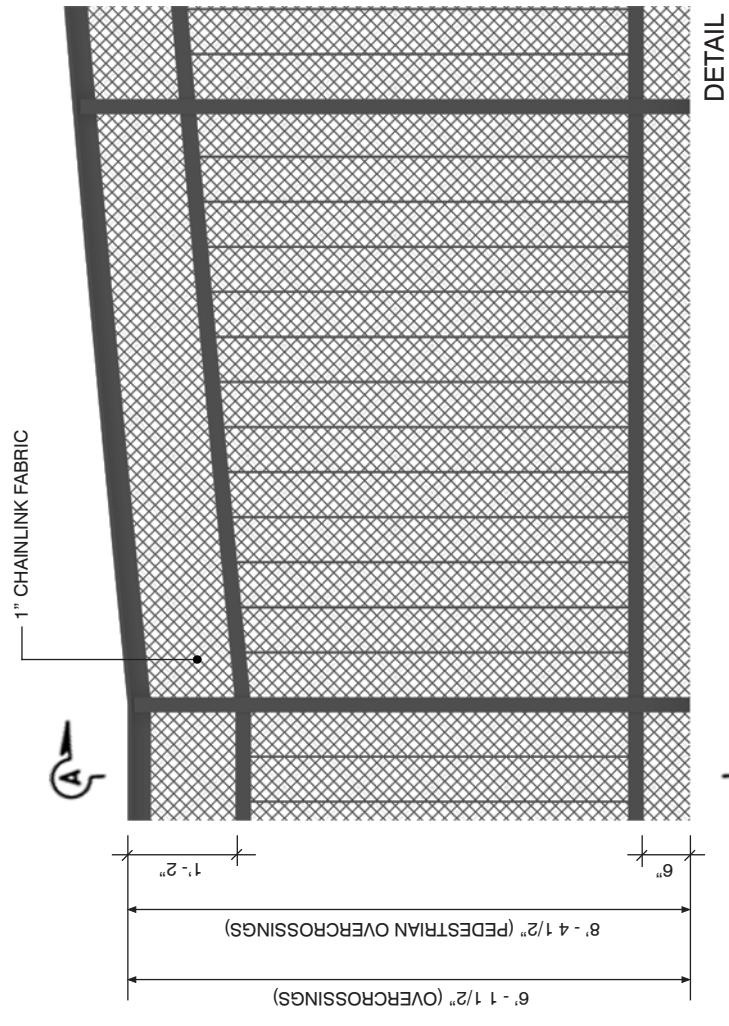
Most bridge replacements will have concrete box girders. The visible face of the girder will be sloped with a deck overhang to provide a shadowed area.



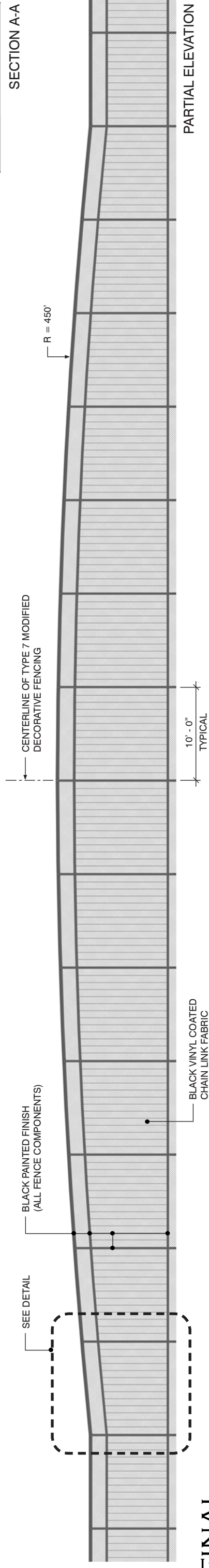
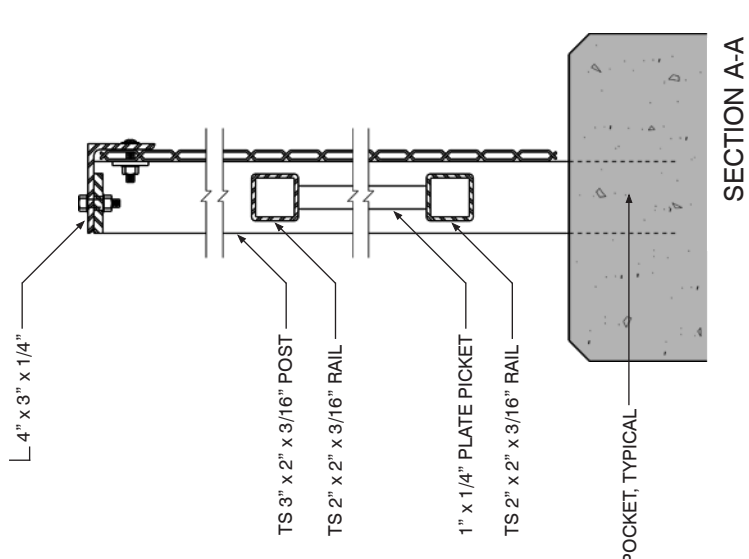
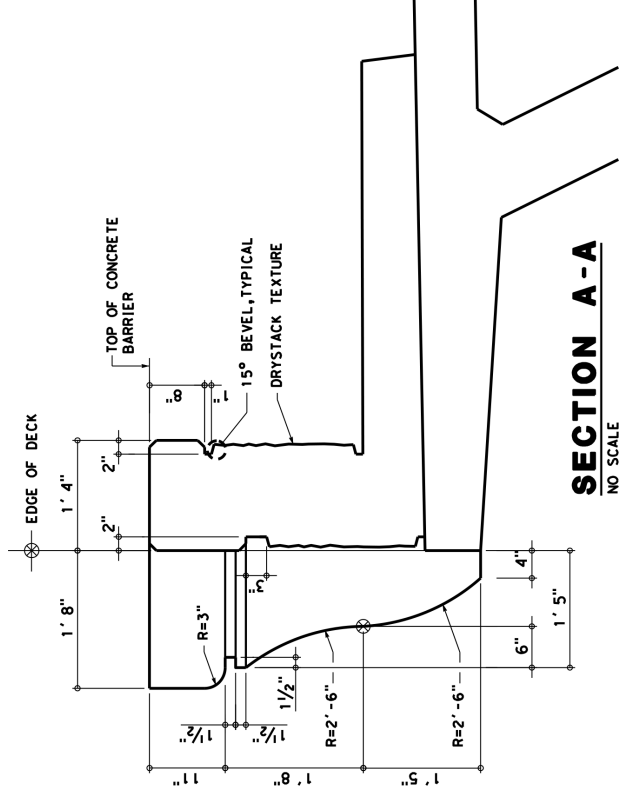
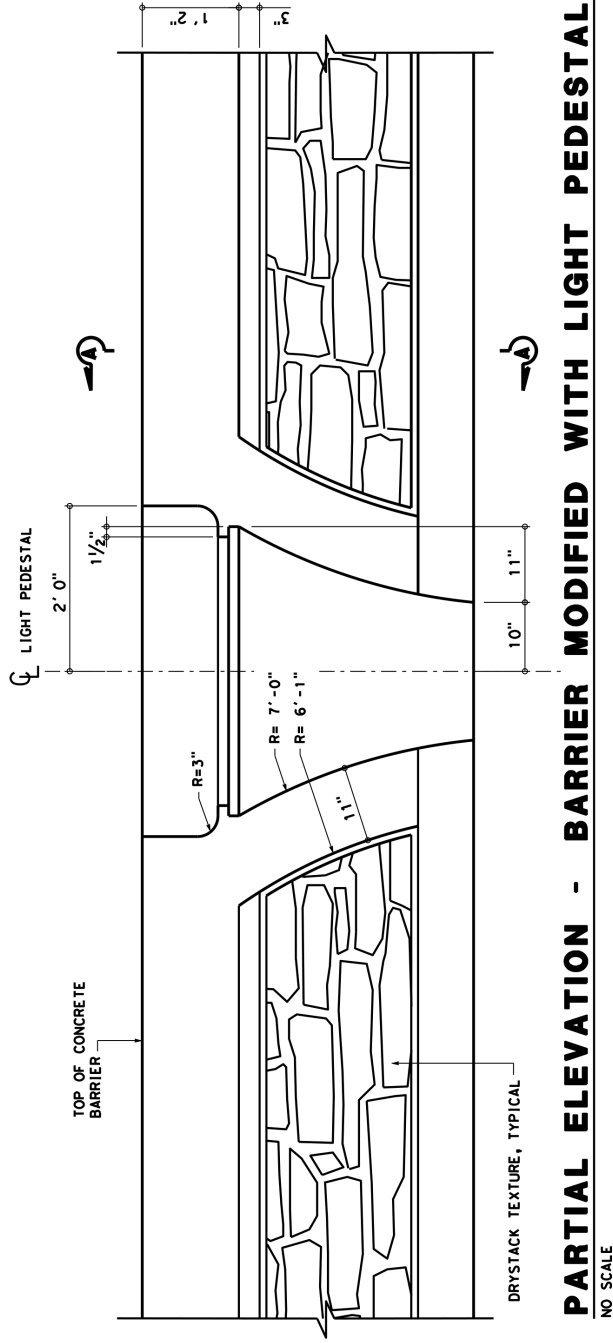
AXONOMETRIC VIEW



PARTIAL ELEVATION



DETAIL



FINAL

4.2 BRIDGE DETAILS



DRystack TEXTURE
 TO MATCH FITZGERALD PATTERN
 #17911 - 1 1/2" RELIEF
 SAN DIEGO DRystack TEXTURE
 PAINT COLORS TO MATCH #'s
 30117, 30215, 36251, AND 37722
 OF FED-STD-595



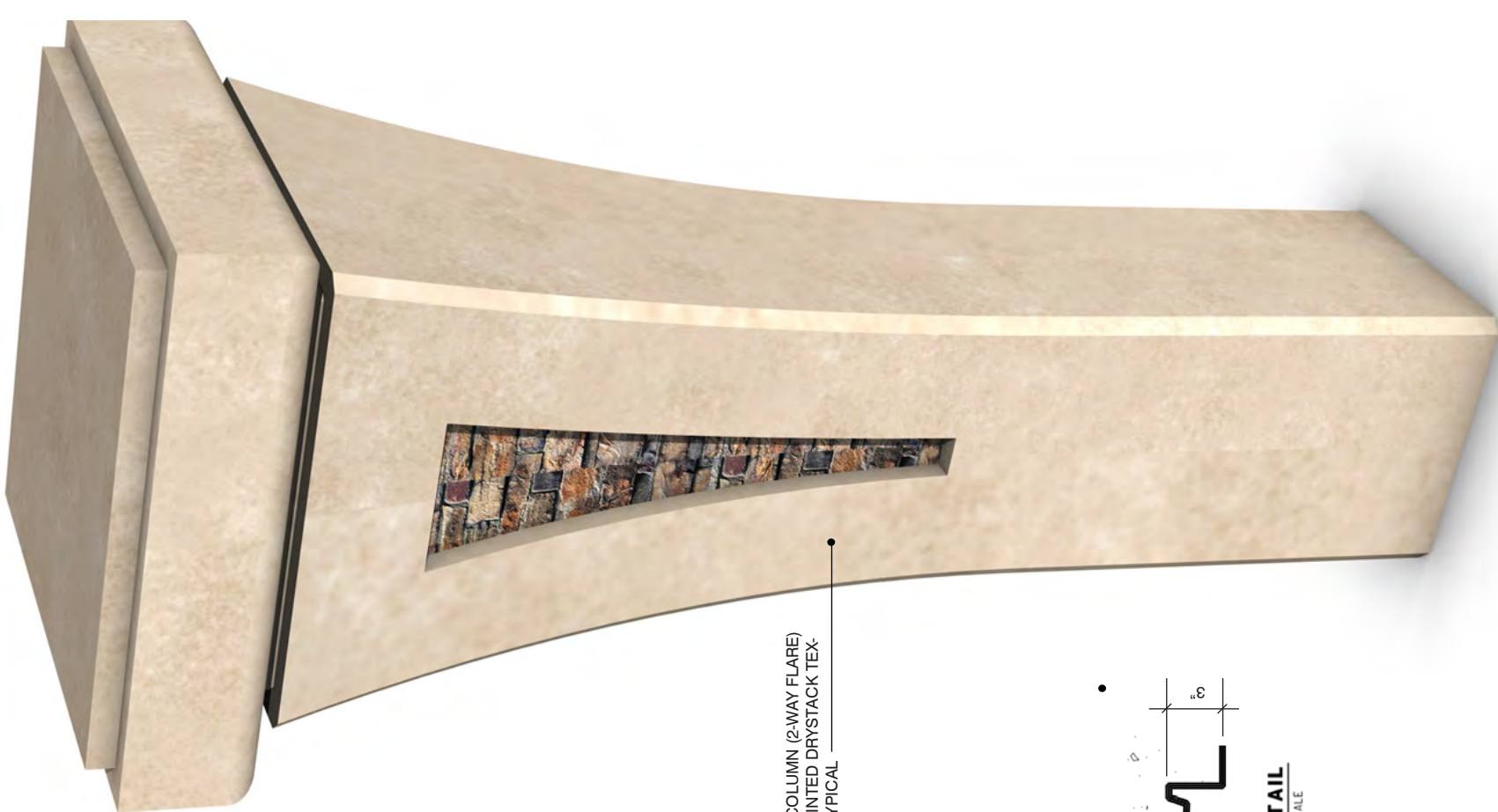
AESTHETIC MOTIF INFLUENCES

FINAL

REV 2 - JUNE 13, 2019

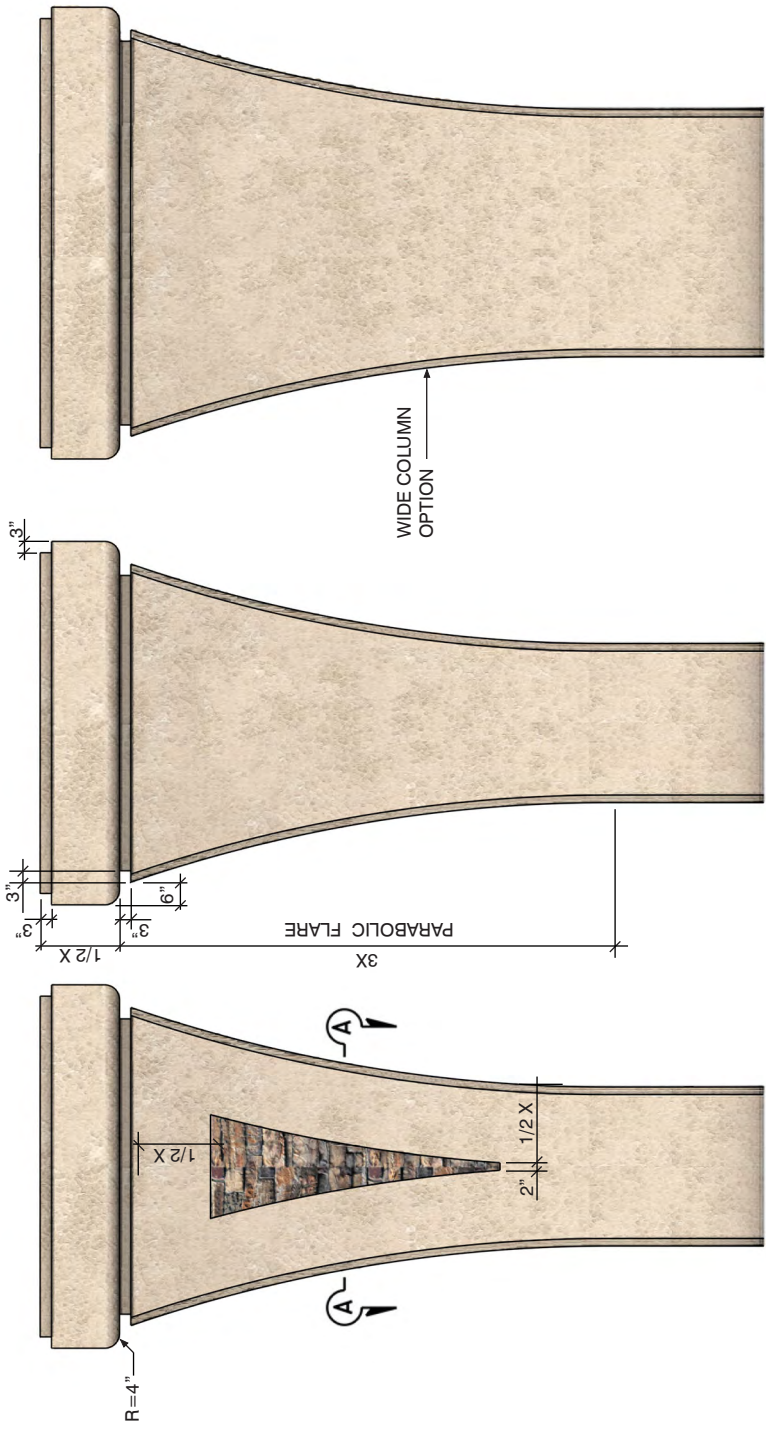
I - 605 CORRIDOR AESTHETIC MASTER PLAN

PAGE 17



AXONOMETRIC VIEW

TYPE 3 COLUMN (2-WAY FLARE)
 WITH PAINTED DRystack TEX-
 TURE, TYPICAL



FINAL

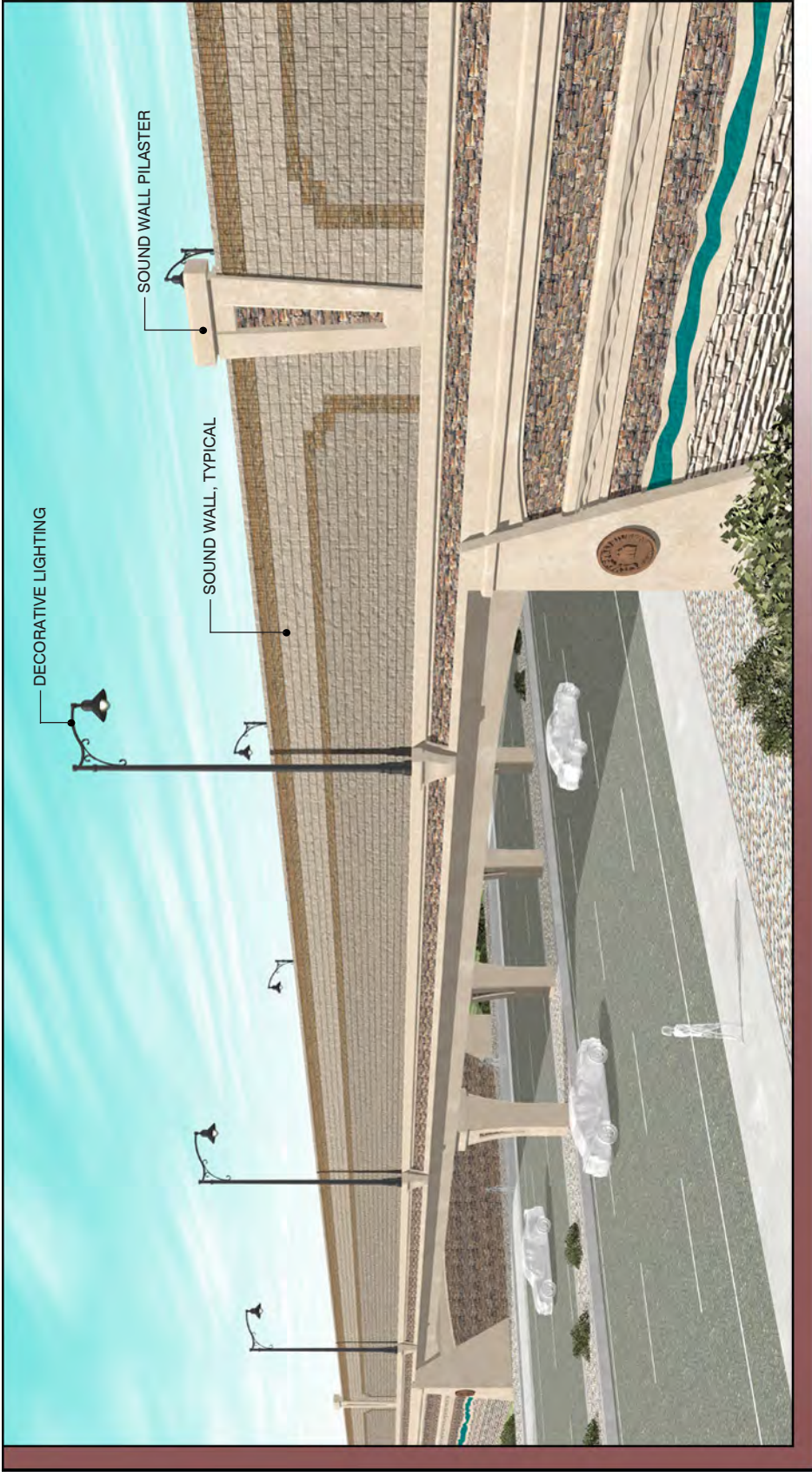
REV 2 - JUNE 13, 2019

I - 605 CORRIDOR AESTHETIC MASTER PLAN

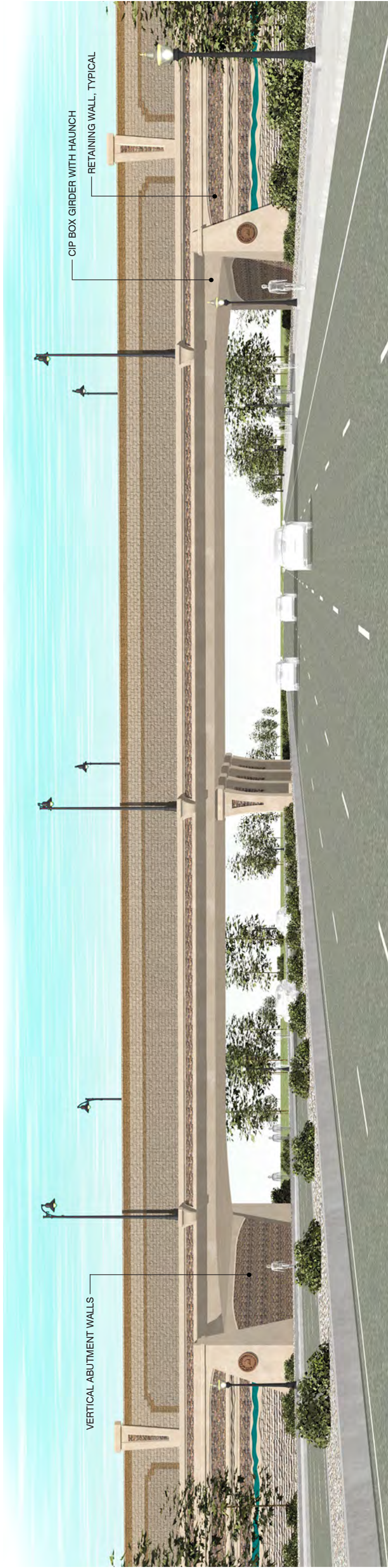
PAGE 17

4.3 UNDERCROSSINGS

The “Aesthetics Committee Master Plan Theme” will be applied to all undercrossings. The architectural treatment consists of integrally colored concrete, painted drystack stone texture, basalt texture, colored horizontal band representing the river, and project fencing and lighting with black finish.

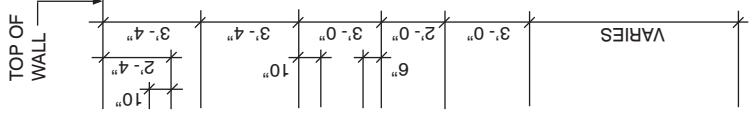
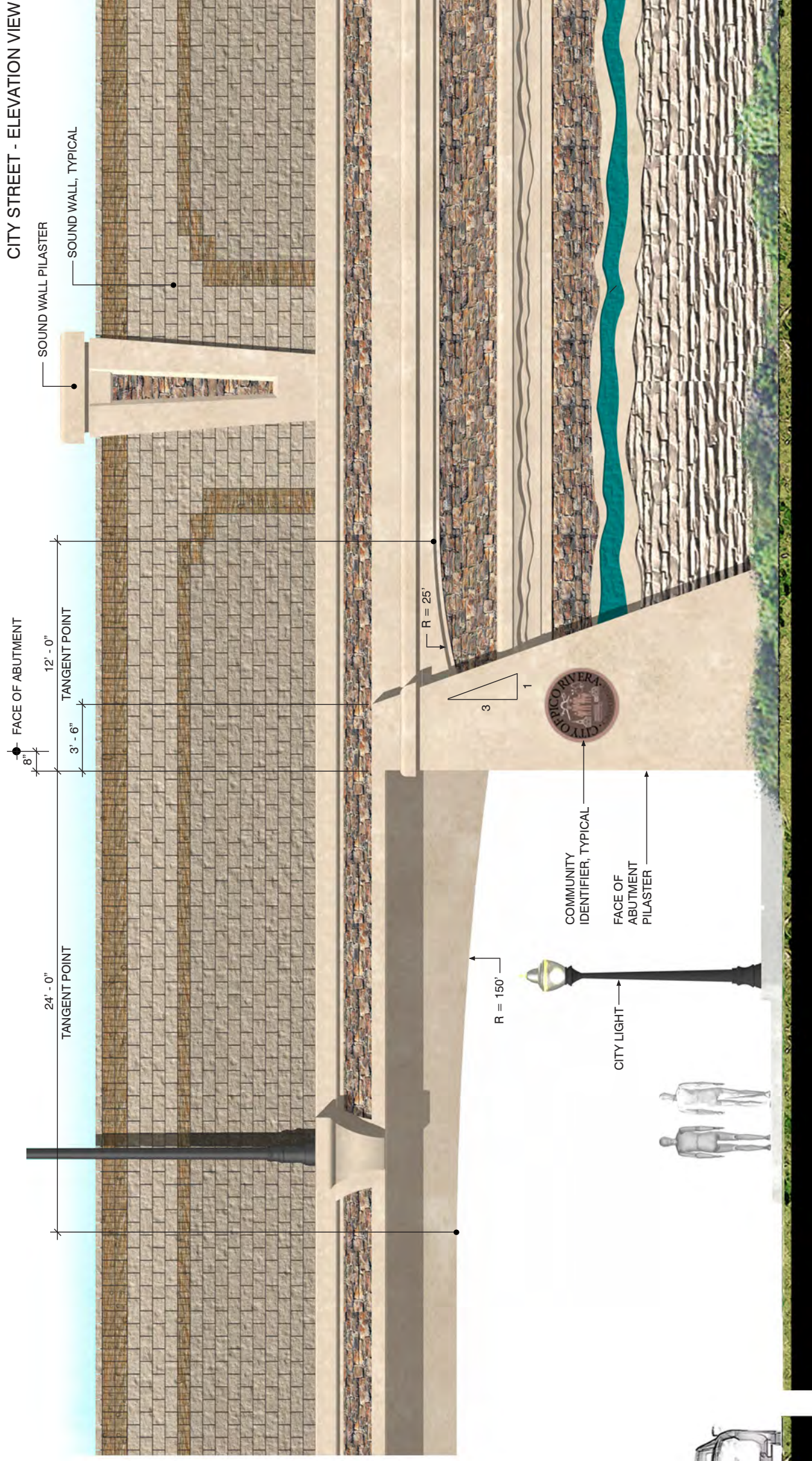
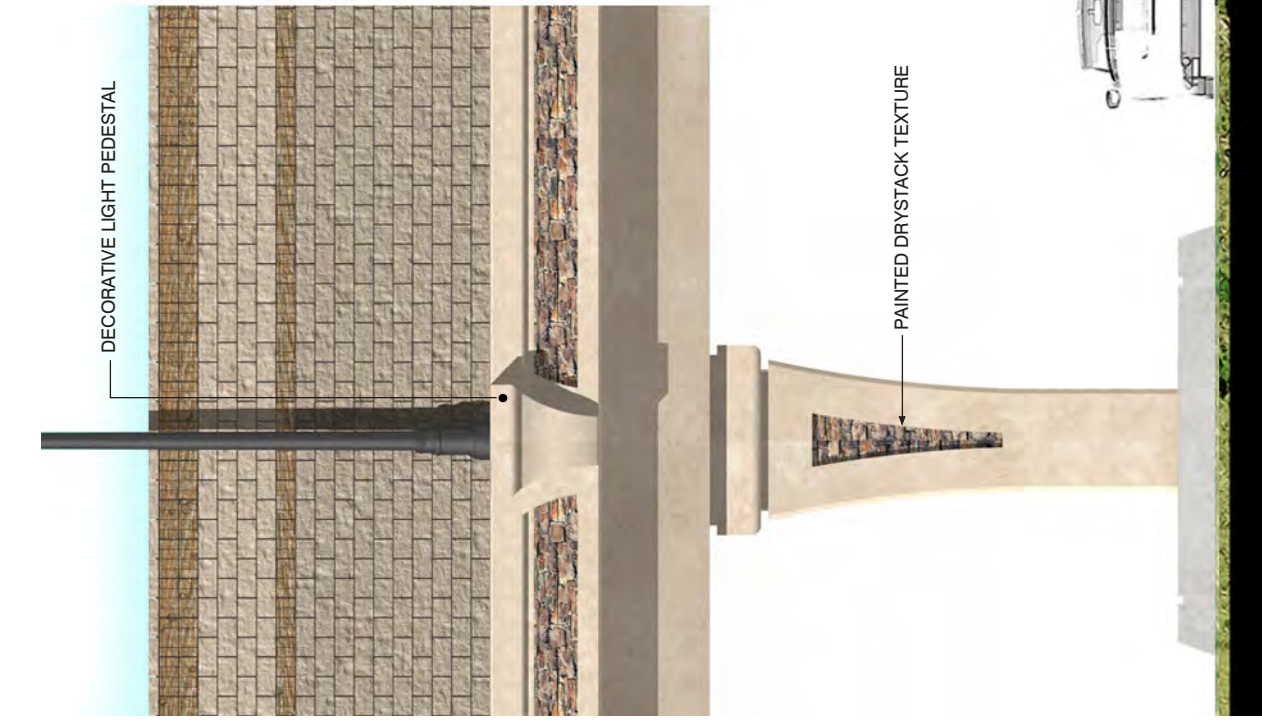
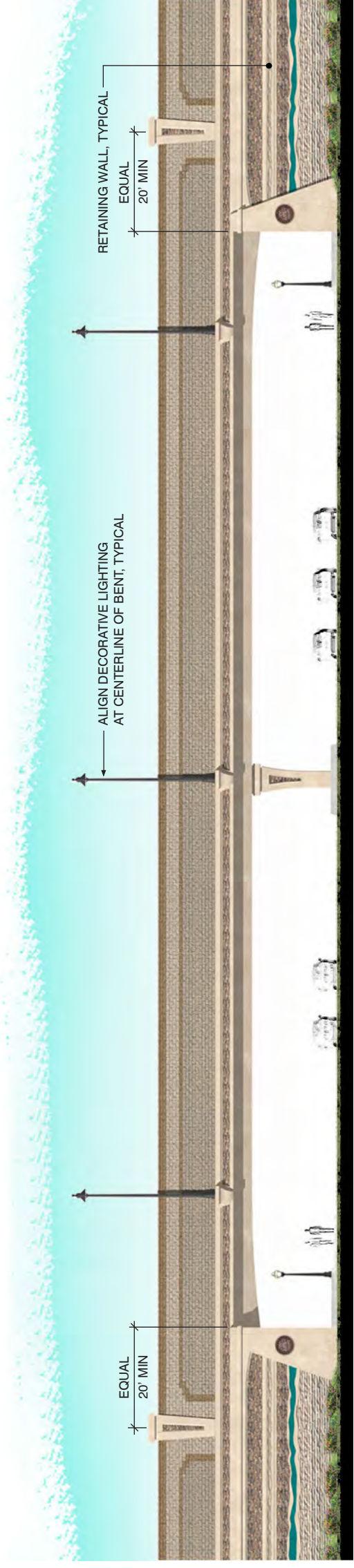


PERSPECTIVE VIEW



CITY STREET - PERSPECTIVE VIEW

4.3 UNDERCROSSINGS



PARTIAL ELEVATION VIEW

FINAL

REV 2 - JUNE 13, 2019

I - 605 CORRIDOR AESTHETIC MASTER PLAN

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4.3 UNDERCROSSINGS

Bridge corner pilasters will be used to frame bridges and provide opportunity to incorporate community graphic castings. Each city along the I-605 Corridor Project will have an opportunity to incorporate their city seal into the pilaster to create a community identifier welcoming travelers and visitors.

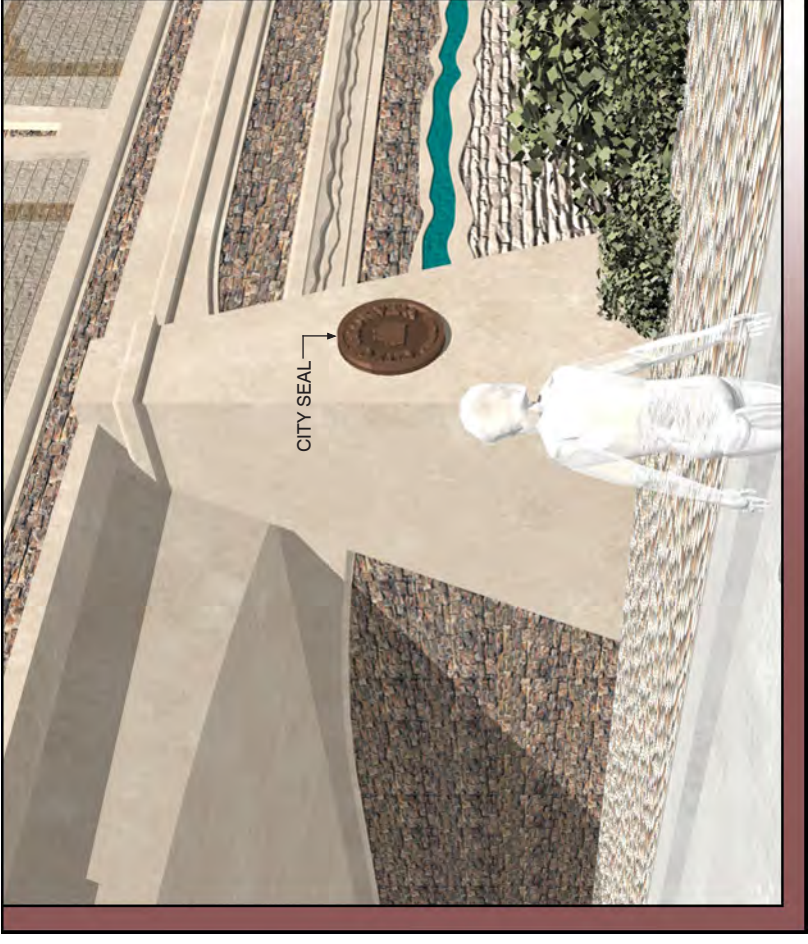
Note: This feature to be negotiated by LA Metro, Caltrans, and the individual cities.



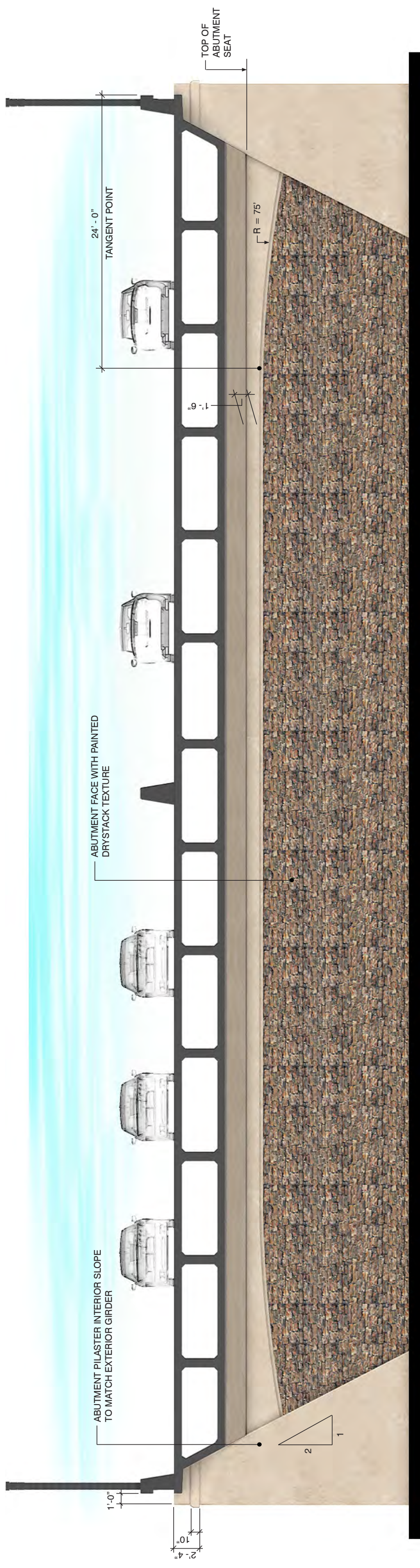
CITY SEAL EXAMPLE



PERSPECTIVE VIEW



PERSPECTIVE VIEW



TYPICAL SECTION

4.3 UNDERCROSSINGS



BIRD'S EYE VIEW

FINAL

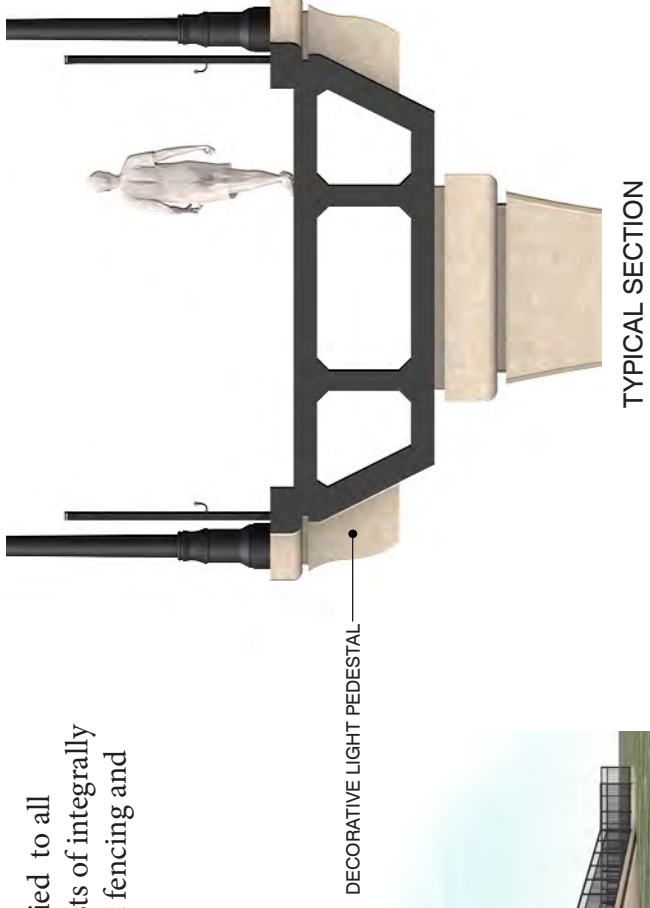
REV 2 - JUNE 13, 2019

I - 605 CORRIDOR AESTHETIC MASTER PLAN

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4.4 PEDESTRIAN OVERCROSSINGS

The "Aesthetics Committee Master Plan Theme" will be applied to all pedestrian overcrossings. The architectural treatment consists of integrally colored concrete, painted drystack stone texture, and project fencing and lighting with black finish.



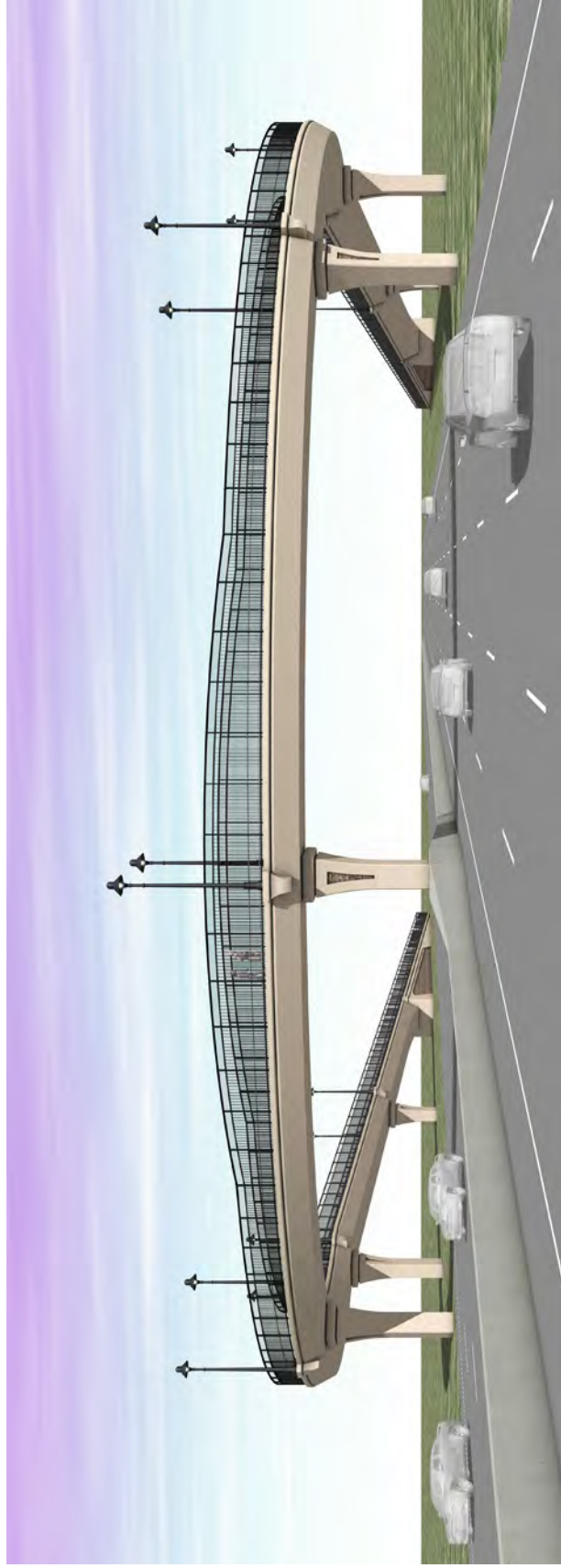
TYPICAL SECTION



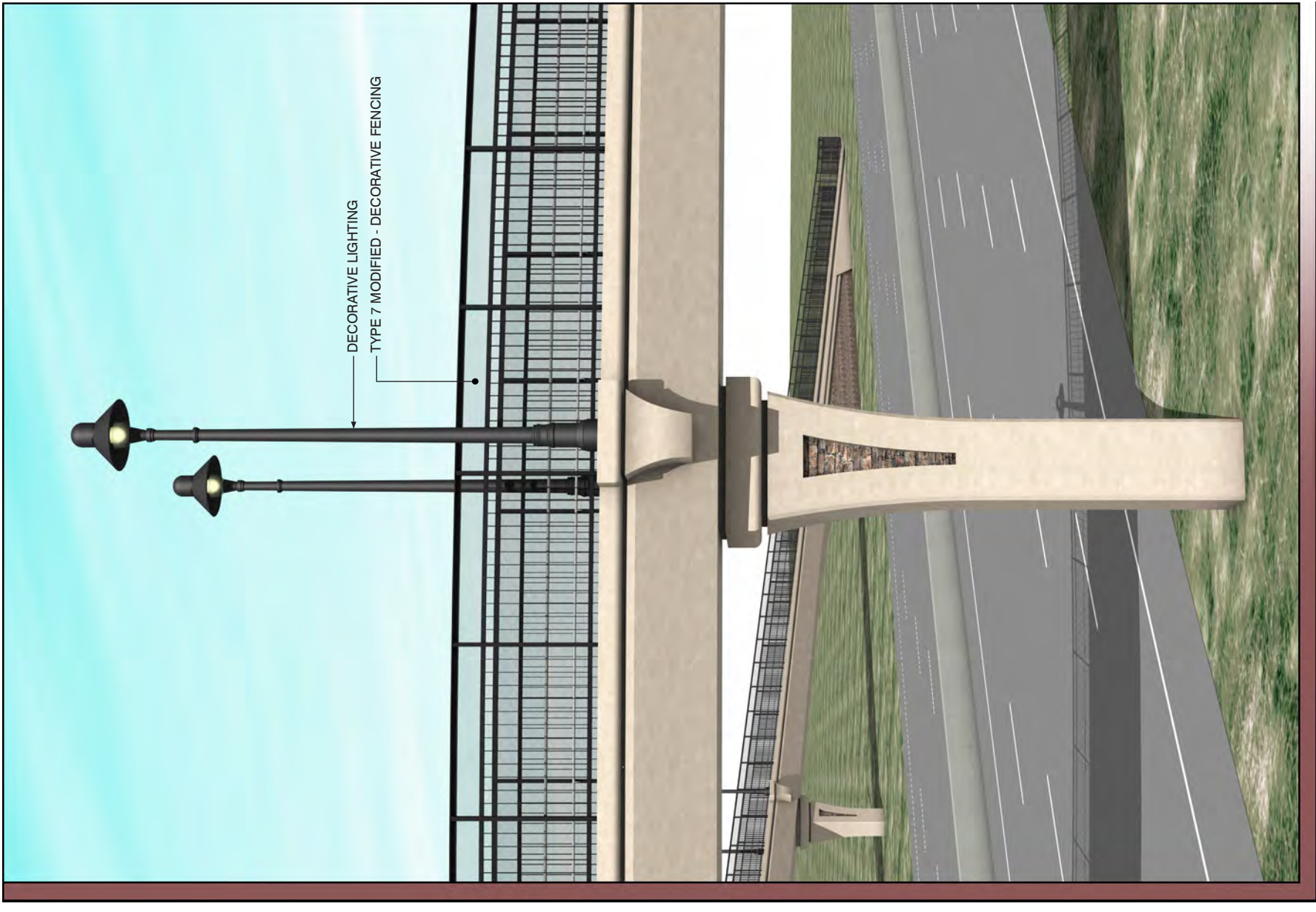
PERSPECTIVE VIEW



PERSPECTIVE VIEW



PERSPECTIVE VIEW



PERSPECTIVE VIEW

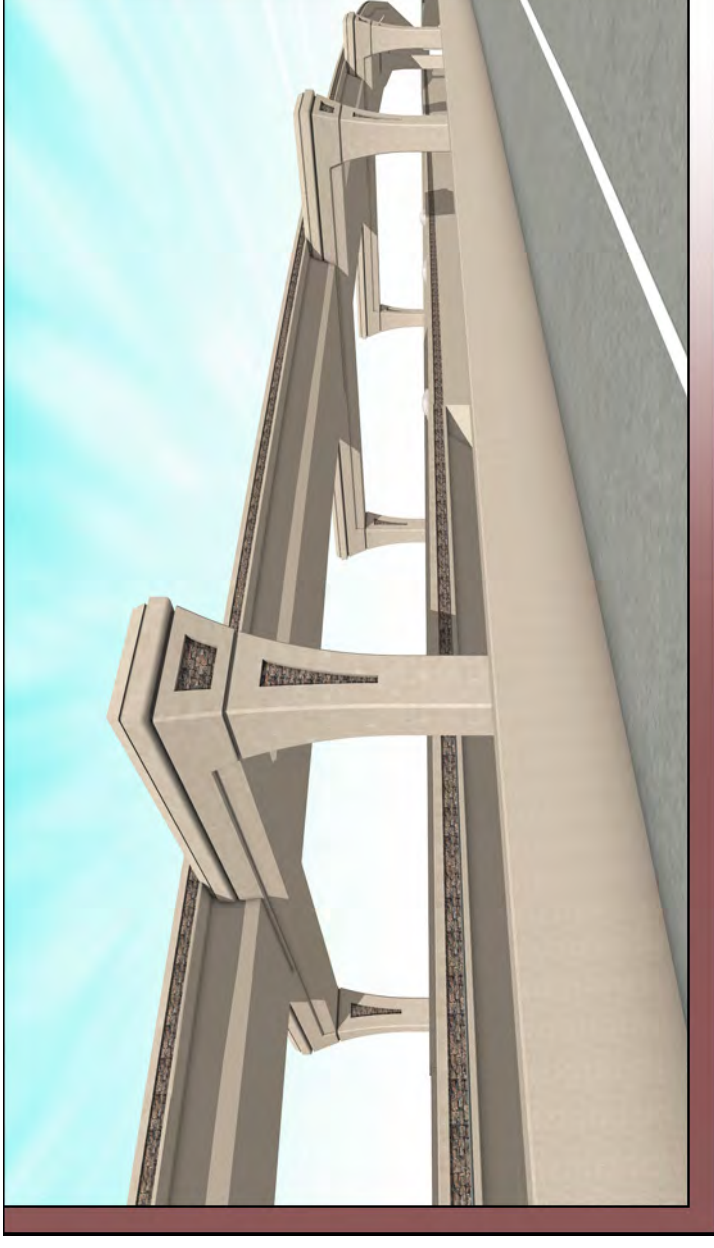
4.5 CONNECTORS

Several interstates and freeways cross paths with the I-605 Corridor. At these intersecting locations, consideration must be given to the convergence of different route design themes. The I-605 Aesthetics Committee will review and approve all designs for interchanges and connectors.

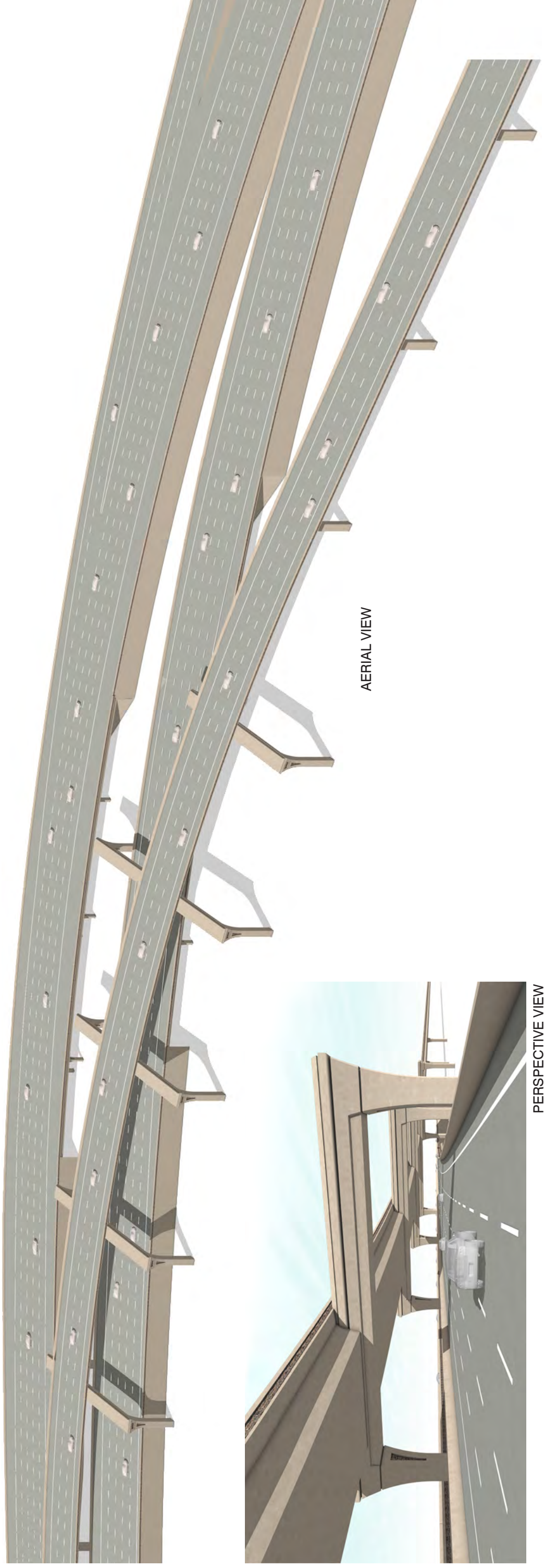


SIDE VIEW

FRONT VIEW



PERSPECTIVE VIEW

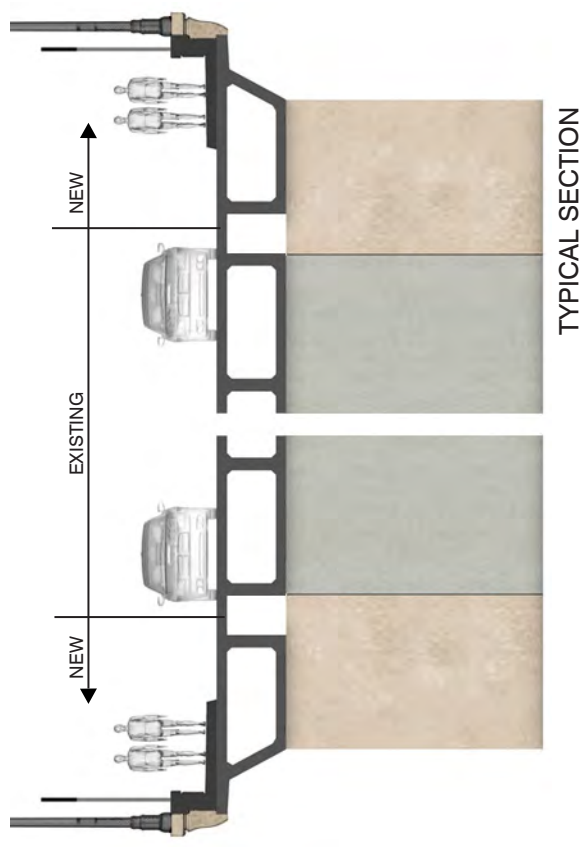
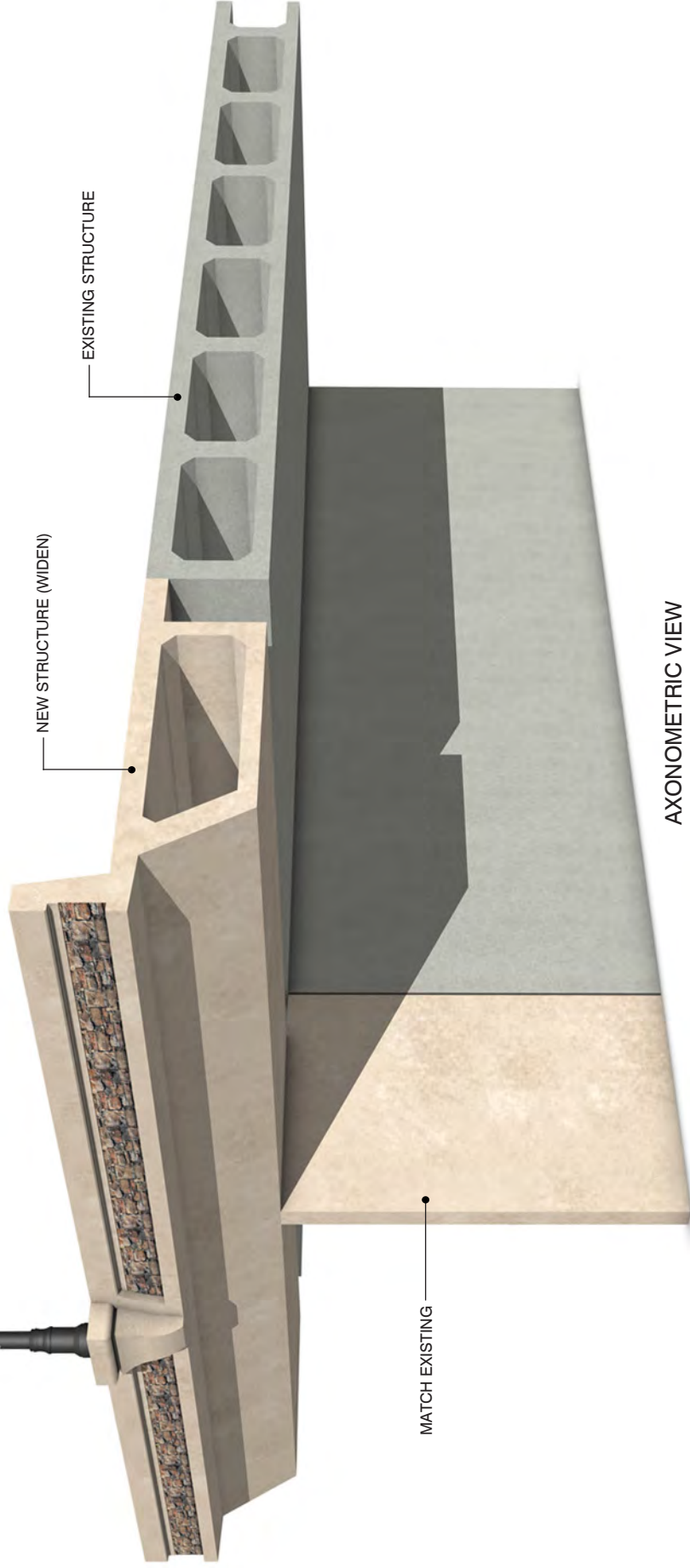
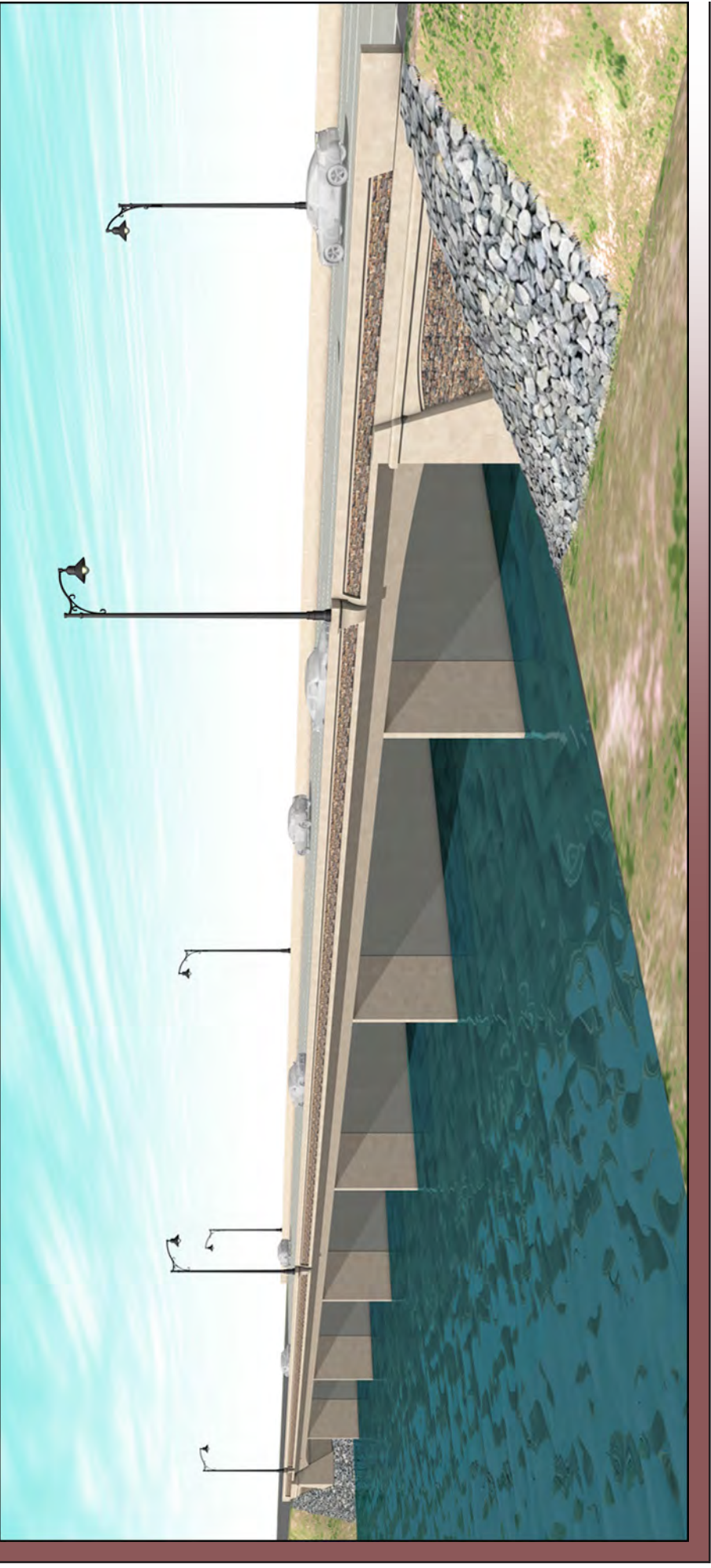
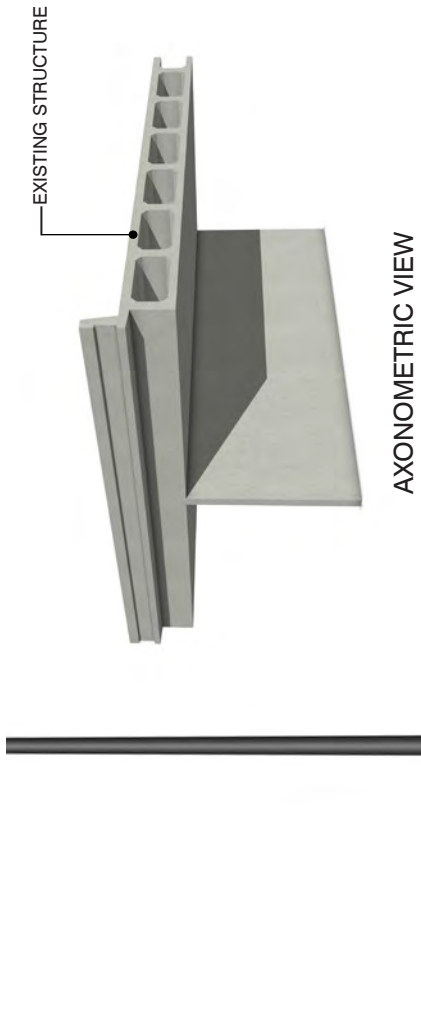


AERIAL VIEW

PERSPECTIVE VIEW

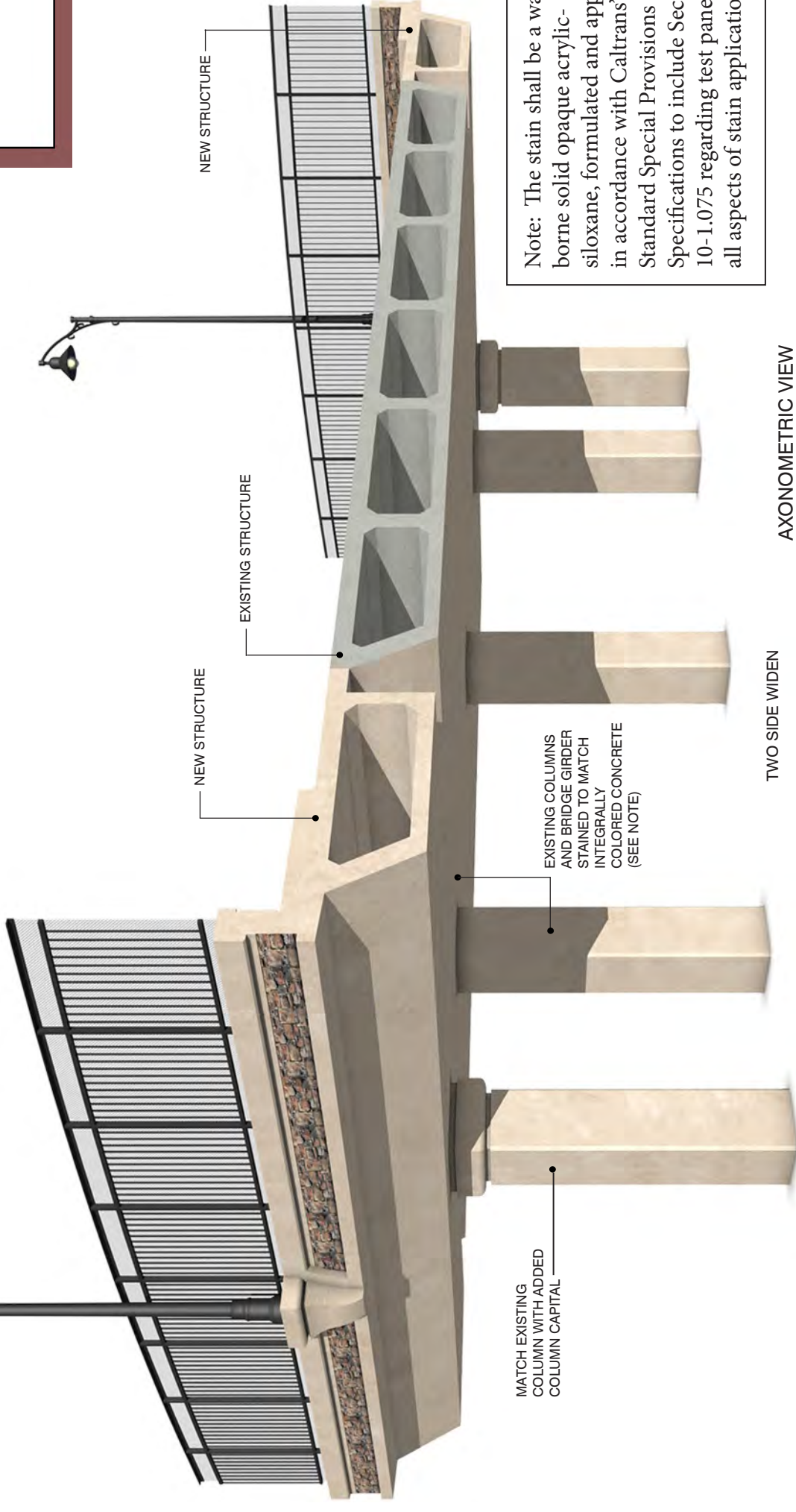
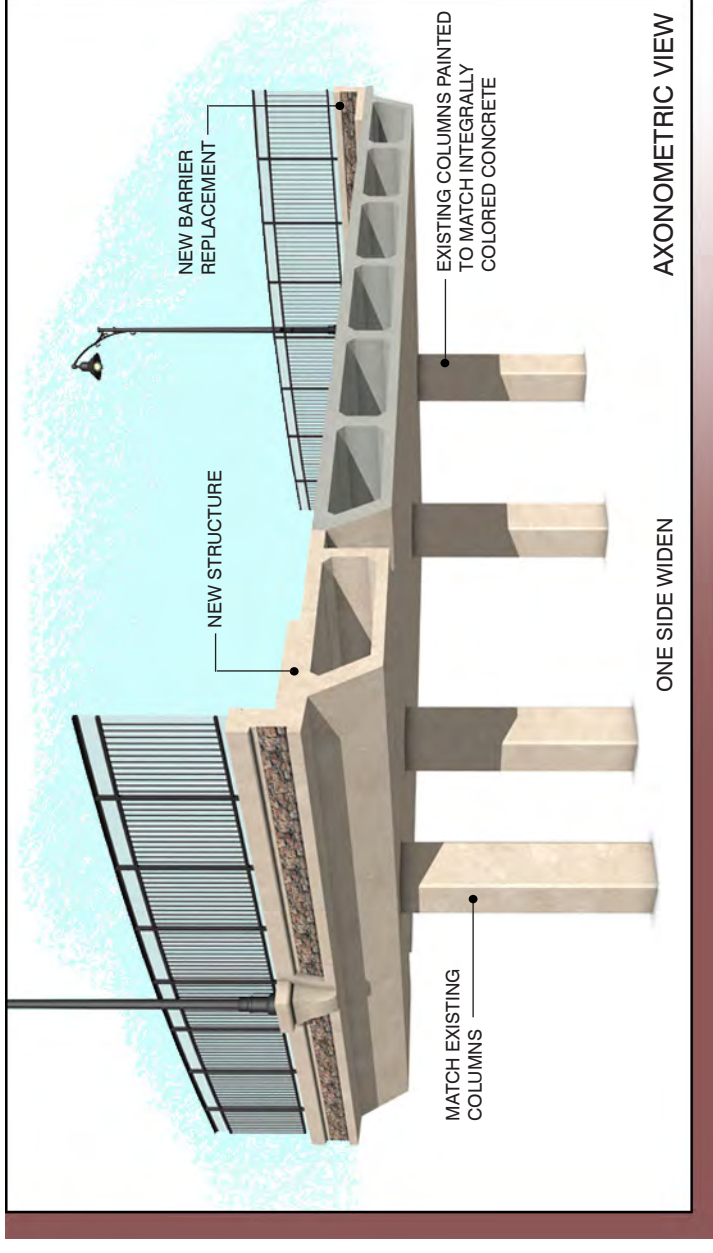
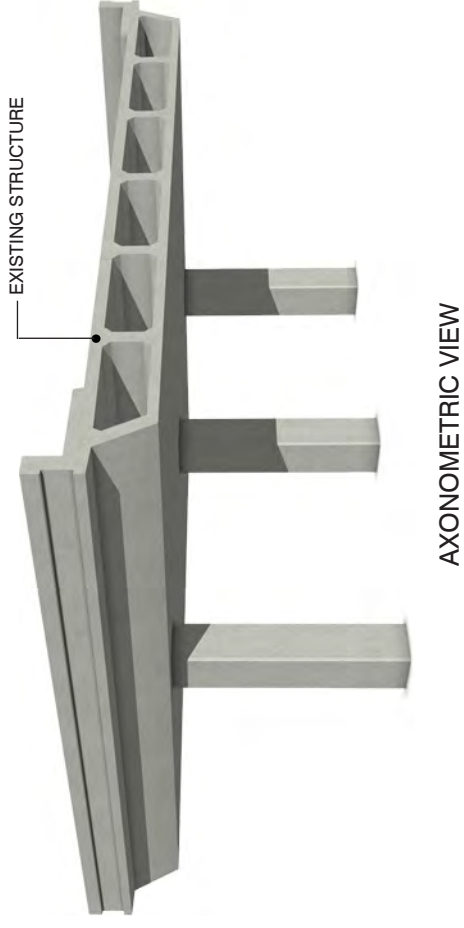
4.6 BRIDGES

The term “bridge” is usually reserved for structures over water courses. Existing bridges along the I-605 Corridor will be widened. The “Aesthetic Committee Master Plan Theme” will be applied to the widened portions of the bridges to incorporate the design theme and unify the structures along the corridor.

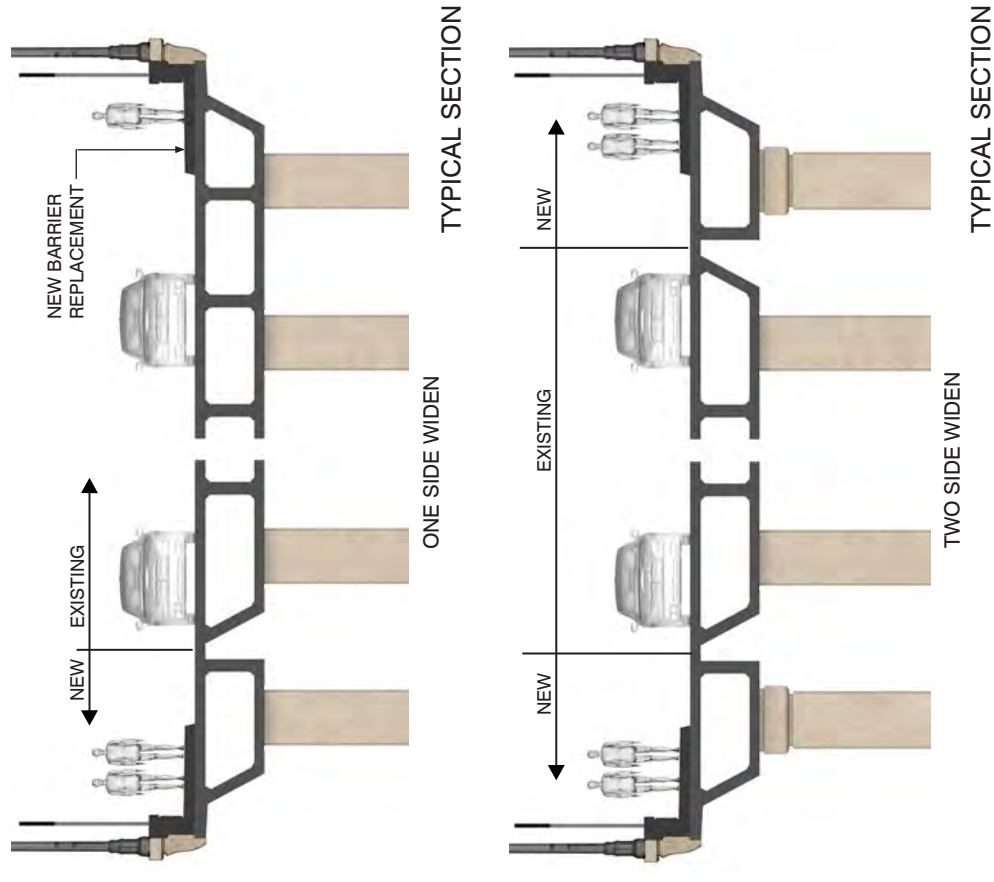


4.7 WIDENING EXISTING STRUCTURES

There will be several locations along the I-605 Corridor where the widening of existing structures will occur. The I-605 Project architectural treatment will be applied to the exterior girder, new bridge barriers, fencing, lighting, etc. to unify the existing structures with the new structures. At locations with one side widened, bridge columns will match the existing structure for visual consistency. At locations with both sides widened, bridge columns will match the existing structure with the addition of a capital.

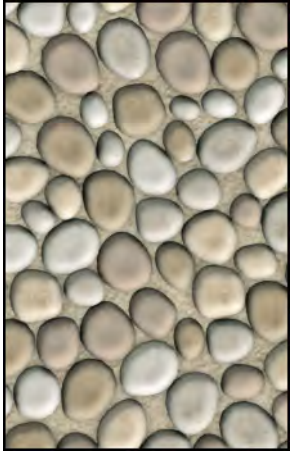


Note: The stain shall be a water-borne solid opaque acrylic-siloxane, formulated and applied in accordance with Caltrans' Standard Special Provisions and Specifications to include Section 10-1.075 regarding test panels and all aspects of stain application.

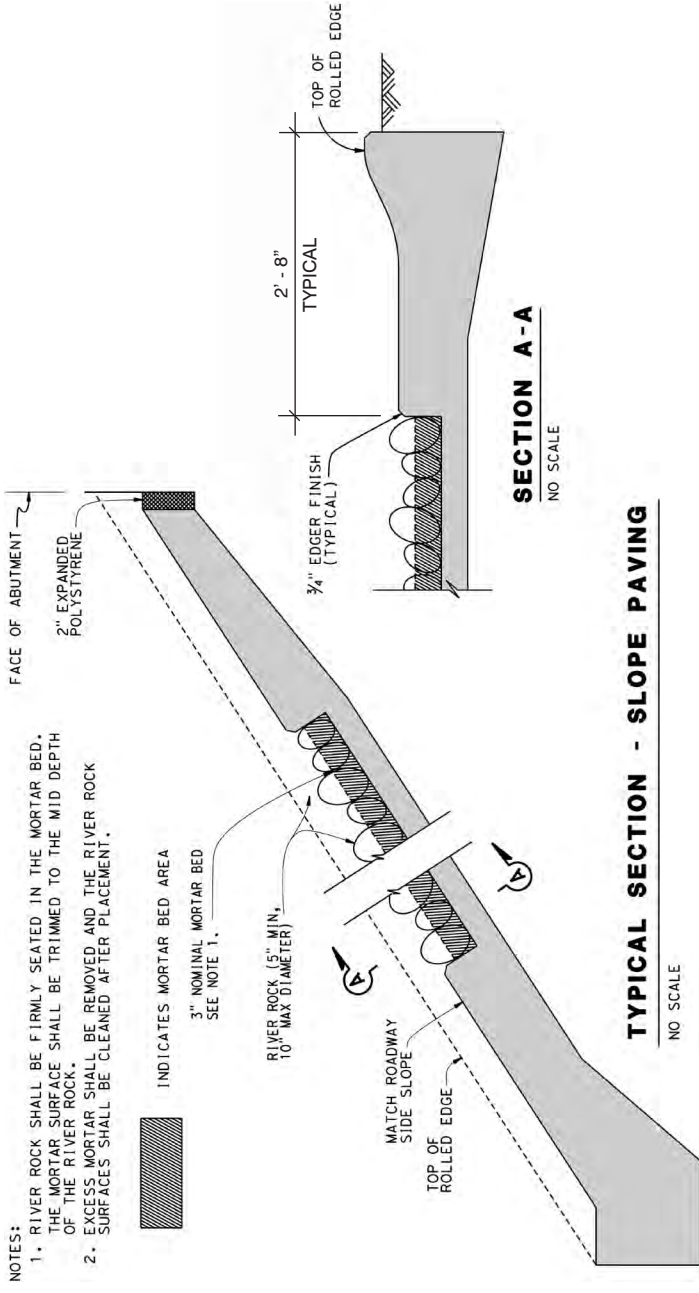


4.8 SLOPE PAVING

Slope paving will incorporate the "Aesthetics Committee Master Plan Theme". This will consist of integrally colored concrete, multi-colored river rock in mortar, with a concrete band flowing horizontally through the rock bed.

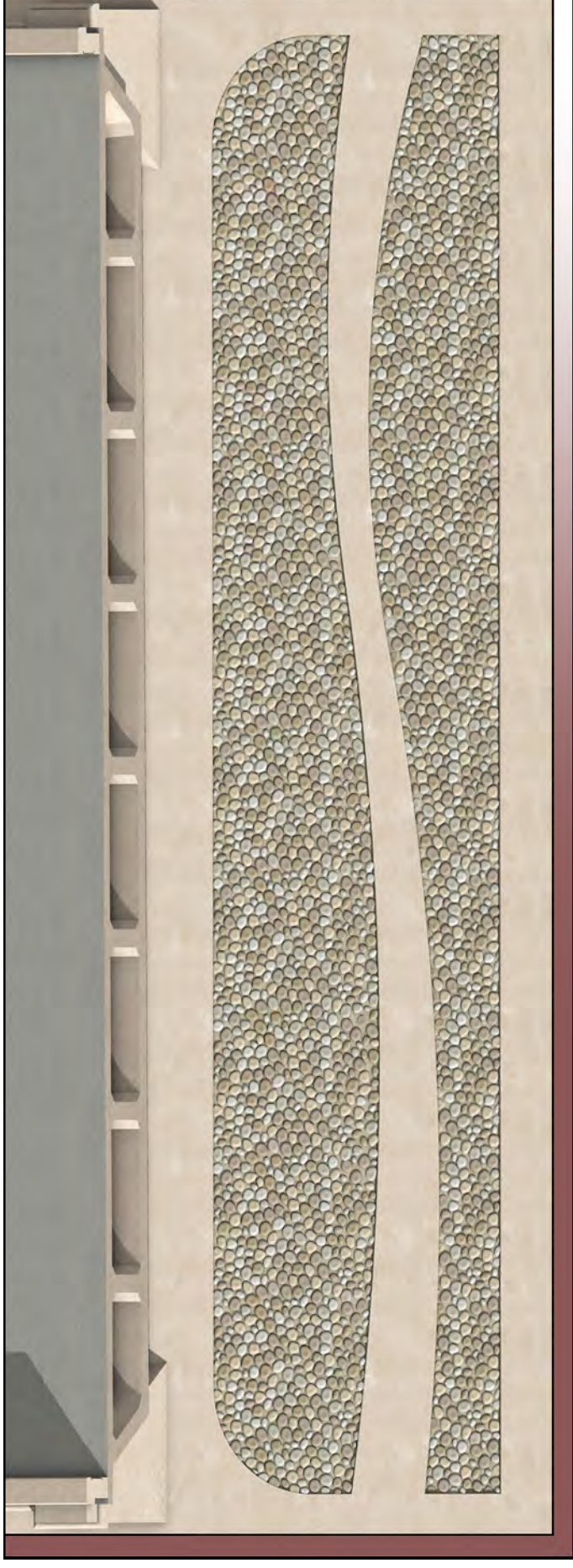


RIVER ROCK

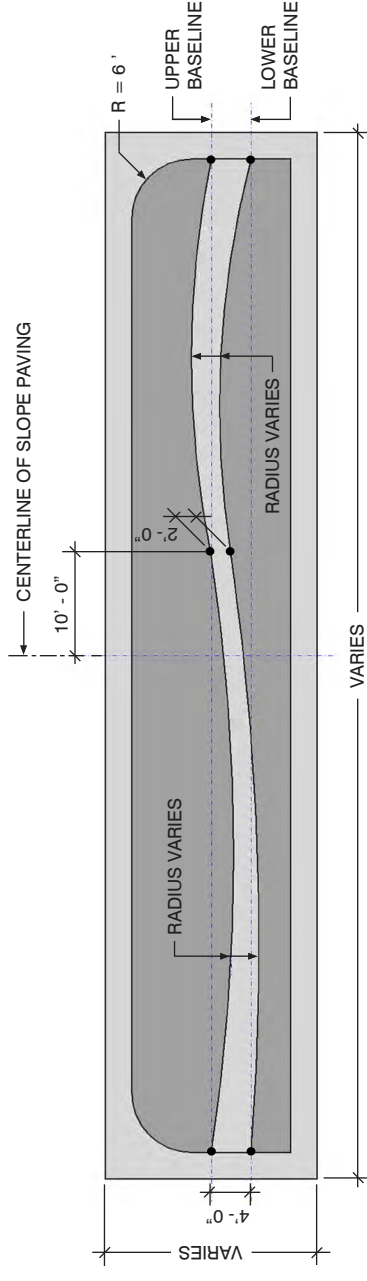


TYPICAL SECTION - SLOPE PAVING

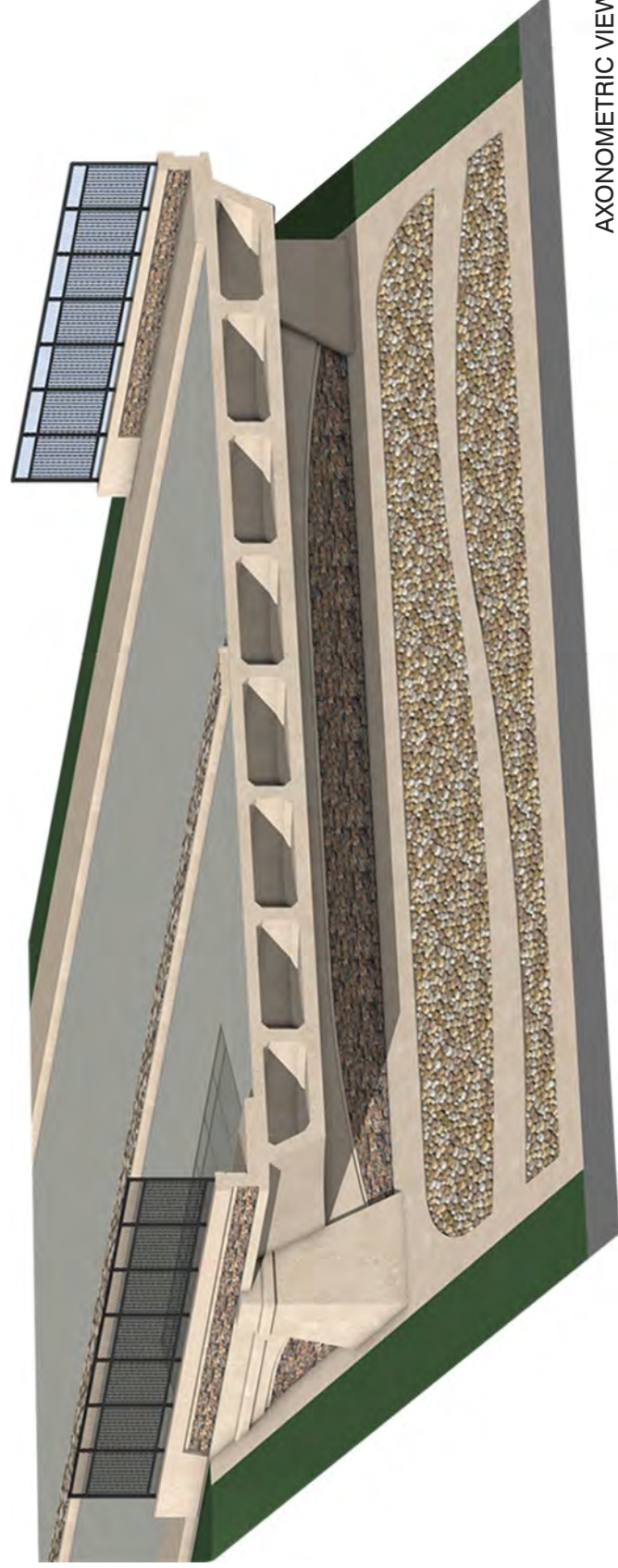
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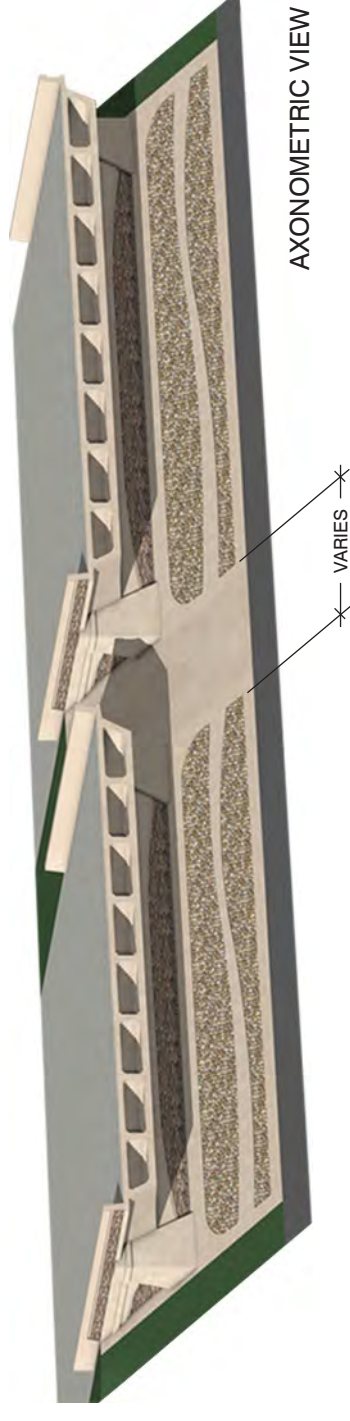
PLAN VIEW



Slope paving improves the overall appearance of the bridge. Used properly, slope paving is an integral element of bridge design which ties together the bridge structure and the surrounding roadway and visually anchors the structure to the environment.



AXONOMETRIC VIEW



AXONOMETRIC VIEW

VARIES

FINAL

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4.9 LIGHTING

ORNAMENTAL CAP

ORNAMENTAL RING

DECORATIVE PENDANT LED LUMINAIRE

STEEL SCROLL

STEEL DECORATIVE ROADWAY ARM

ORNAMENTAL CAP



Lighting is an important feature for a bridge structure both in its functionality and appearance. When selecting lighting fixtures, it is important to not only enhance the design of the structure, but also to compliment the existing lighting from the surrounding areas. For the I-605 CMP, a family of lighting fixtures have been proposed that are simple and elegant in design, yet similar to existing lighting along the I-605 corridor to unify the communities. City to negotiate new street lighting.

DECORATIVE POST TOP LED LUMINAIRE

ORNAMENTAL RING

TAPERED STEEL POLE

CAST ALUMINUM BASE

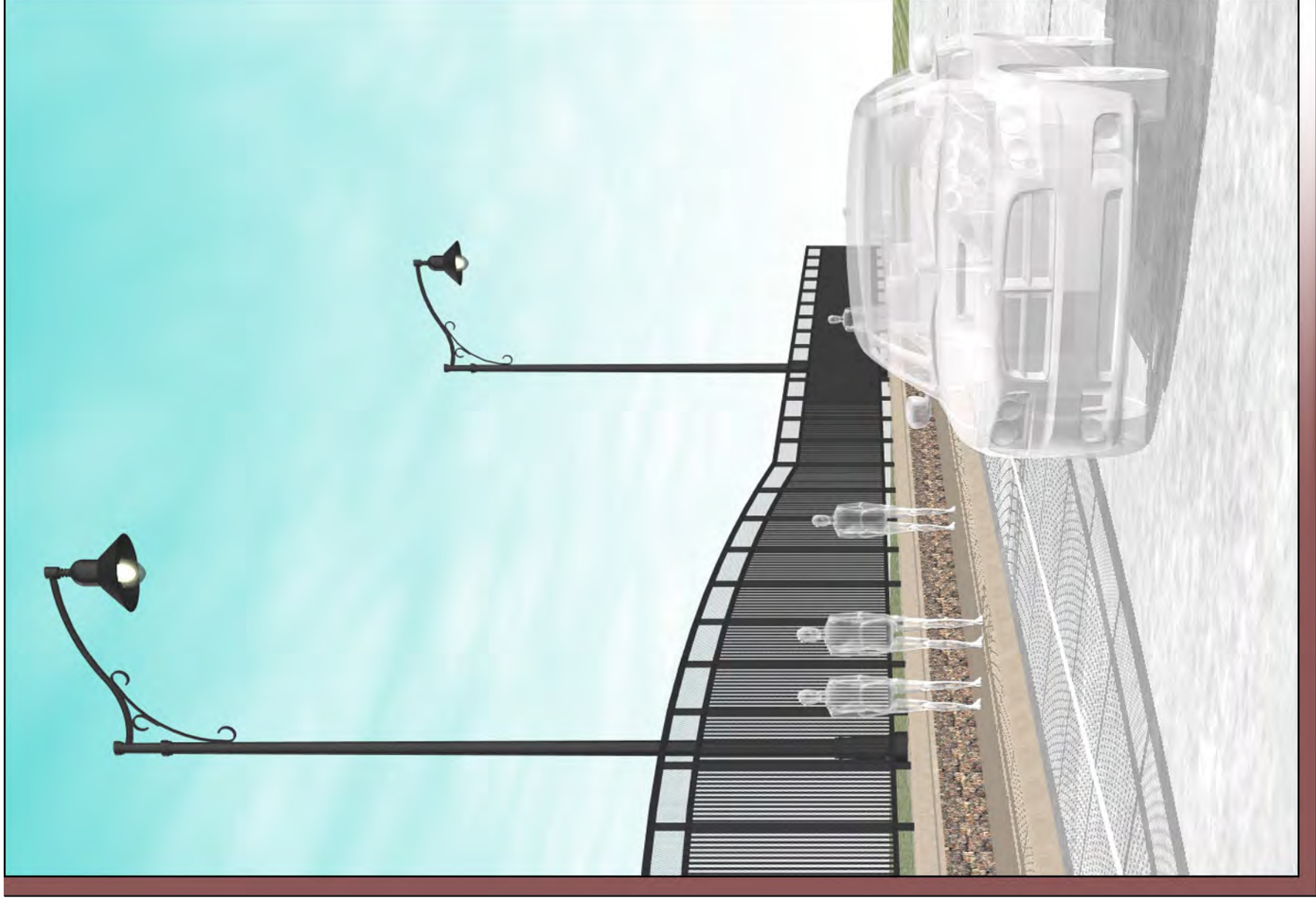
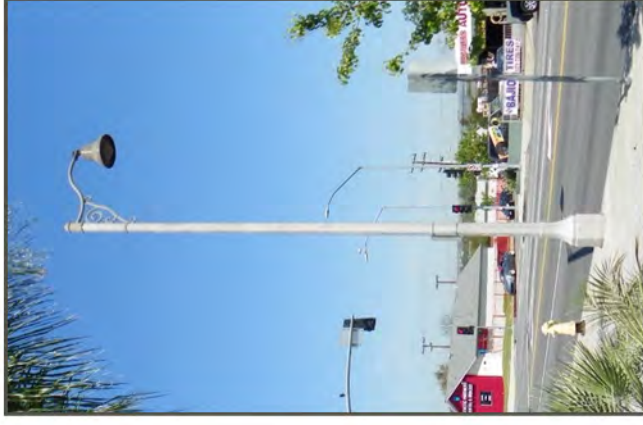
MINIMUM 3'-0"

1'-6" TYP

STRUCTURAL LIGHTING

STREET LIGHTING

Note: All light post components to receive a black powder coated finish



FINAL

REV 2 - JUNE 13, 2019

I - 605 CORRIDOR AESTHETIC MASTER PLAN

PERSPECTIVE VIEW

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4.10 RETAINING WALLS

All new retaining walls throughout the I-605 Corridor will have consistent architectural treatment. The textures and patterns will be consistently used on all wall types, whether they are MSE, cast in place, or other wall types.

The “Aesthetics Committee Master Plan Theme” will be applied to all retaining walls and will be a combination of: integrally colored concrete, painted drystack stone texture, basalt texture, and in featured or taller walls a colored horizontal “river” band.

Longer retaining walls may have vertical breaks to minimize monotony and add visual interest.



DRystack TEXTURE

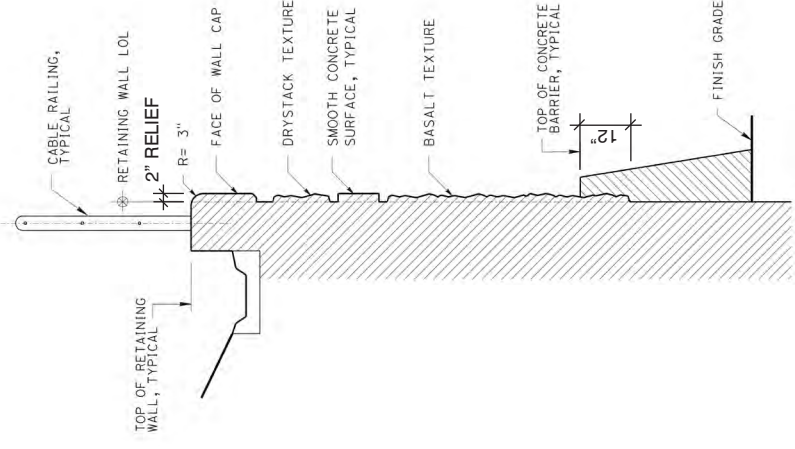
TO MATCH FITZGERALD PATTERN #17911 - 1 1/2" RELIEF
SAN DIEGO DRystack TEXTURE PAINT COLORS TO MATCH # s 30117, 30215, 36251, AND 37722 OF FED-STD-595



BASALT TEXTURE

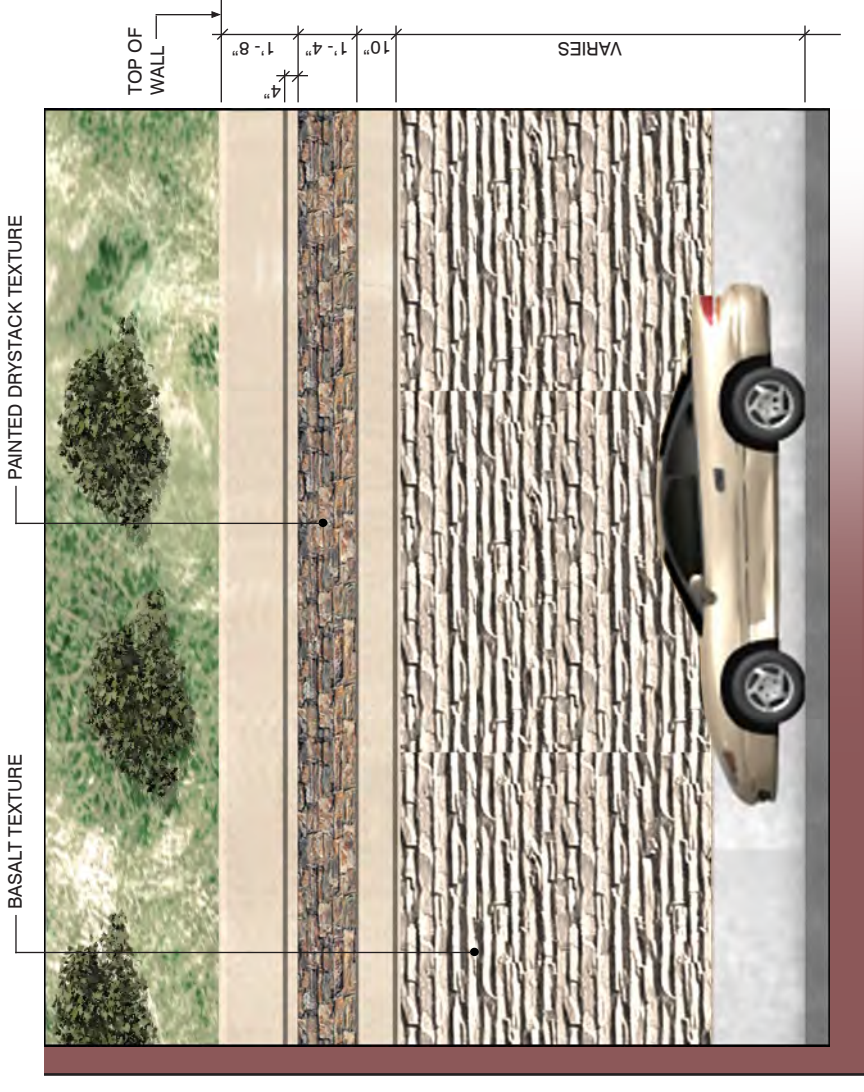
TO MATCH FITZGERALD PATTERN #17928 - 2" RELIEF
WESTERN BASALT TEXTURE (HORIZONTAL ORIENTATION)

Note: Cable Railings have been eliminated from renderings for clarity purposes.



TYPICAL SECTION

NO SCALE



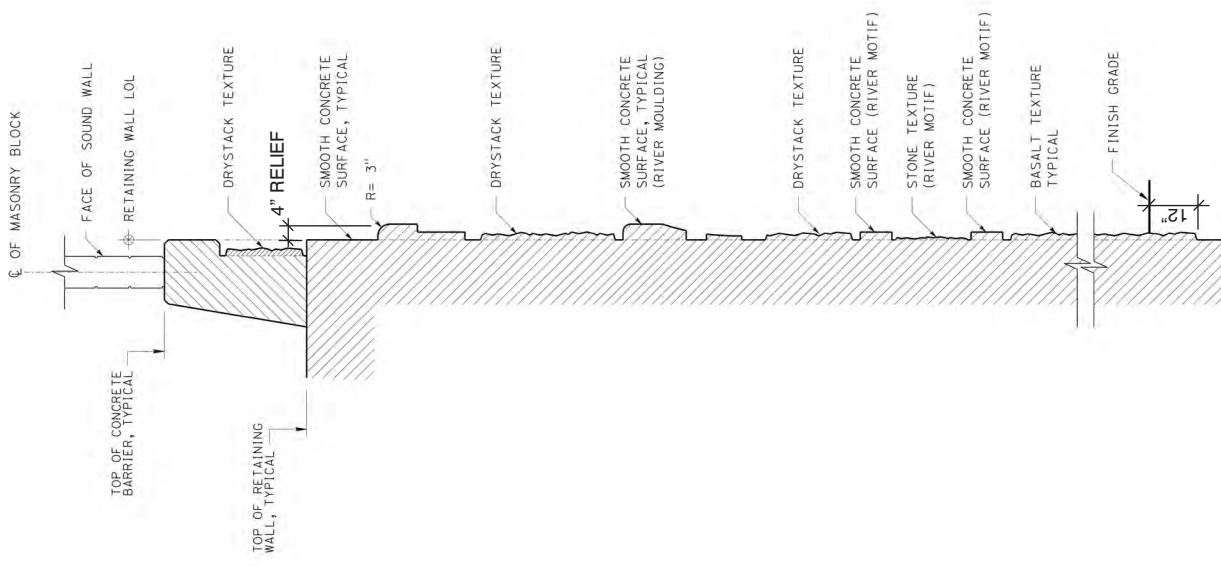
PARTIAL ELEVATION VIEW



BIRD'S EYE - PERSPECTIVE VIEW

4.10 RETAINING WALLS

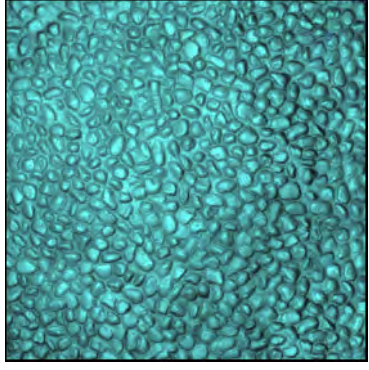
Along the I-605 Corridor, a majority of new retaining walls will be seen from adjacent communities. Where right-of-way space allows maintenance access, vegetation can be planted to “soften” wall surfaces. The Contractor will construct vine planting cutout areas at the base of retaining walls. The vines will eventually grow and extend above the sound wall and will be seen from the freeway side.



TYPICAL SECTION
NO SCALE



DRystack TEXTURE
TO MATCH FITZGERALD PATTERN #17911 - 1 1/2" RELIEF
SAN DIEGO DRystack TEXTURE PAINT COLORS TO MATCH #'S 30117, 30215, 36251, AND 37722 OF FED-STD-595



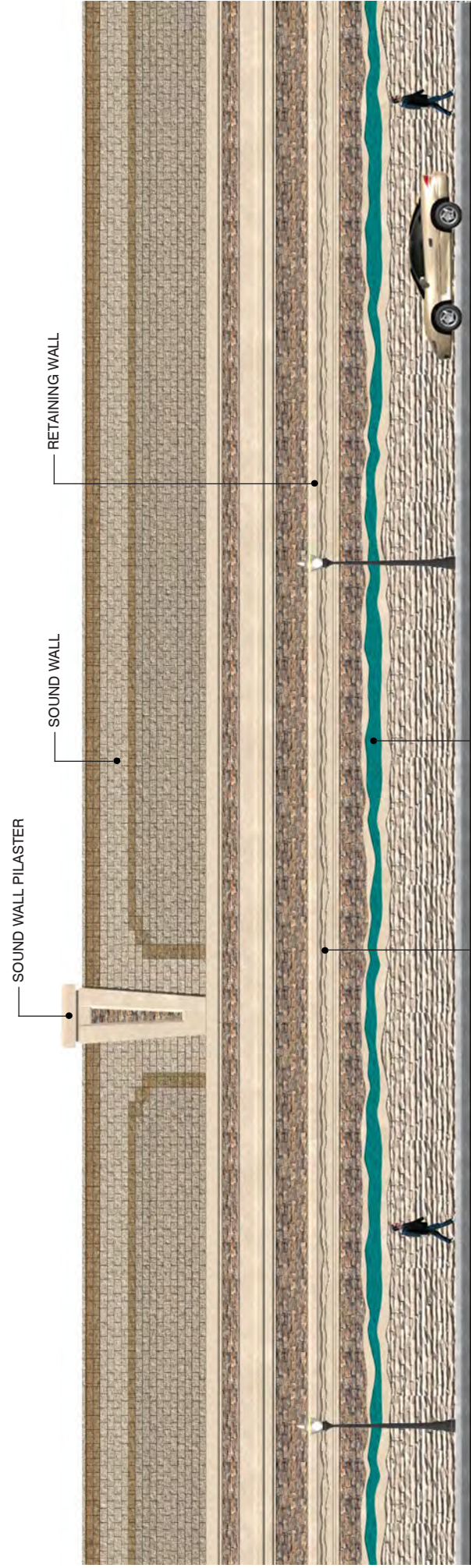
STONE TEXTURE
TO MATCH FITZGERALD PATTERN #16983 - 3/8" RELIEF ROUNDSTONE TEXTURE PAINT COLOR TO MATCH #35193 OF FED-STD-595



BASALT TEXTURE
TO MATCH FITZGERALD PATTERN #17928 - 2" RELIEF WESTERN BASALT TEXTURE (HORIZONTAL ORIENTATION)

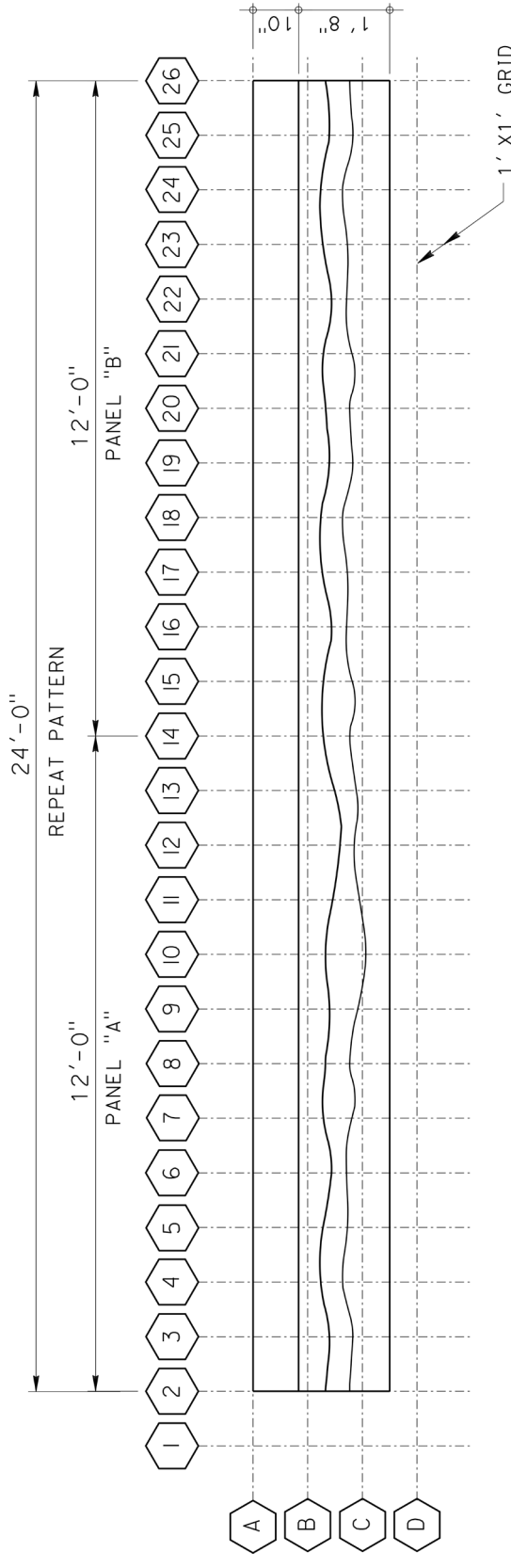
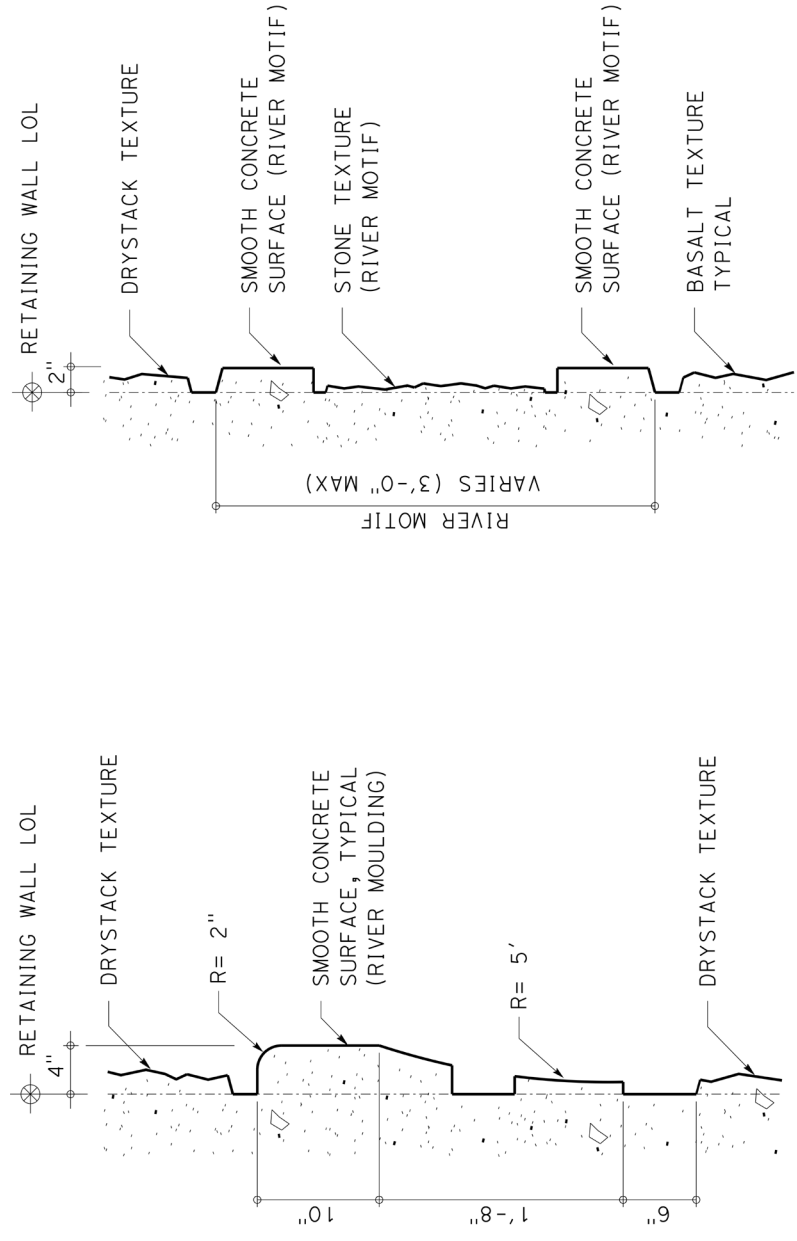


PERSPECTIVE VIEW



PARTIAL ELEVATION VIEW

4.11 RETAINING WALL DETAILS



ELEVATION - RIVER MOULDING PATTERN

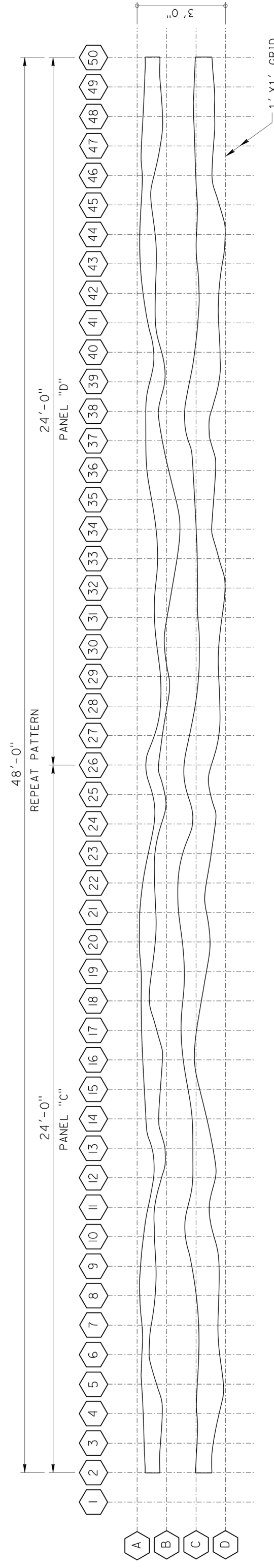
NO SCALE

RIVER MOULDING DETAIL

NO SCALE

RIVER MOTIF DETAIL

NO SCALE



ELEVATION - RIVER MOTIF PATTERN

NO SCALE

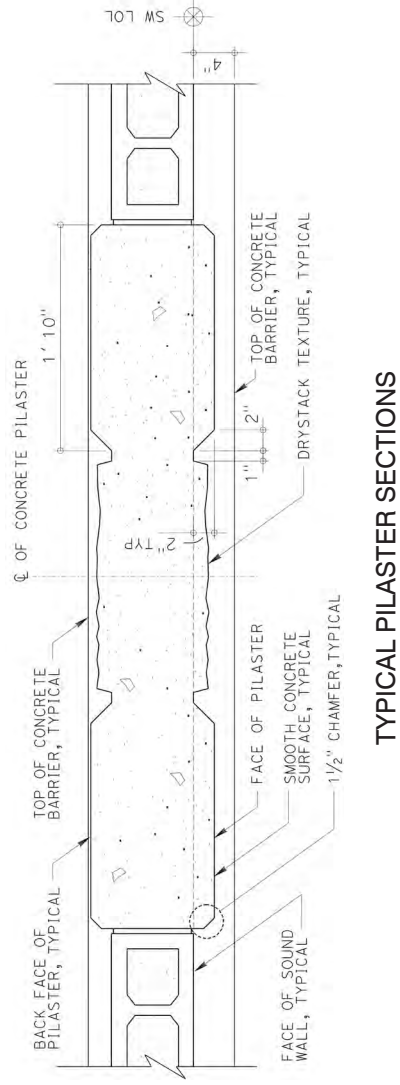
FINAL

4.12 SOUND WALLS

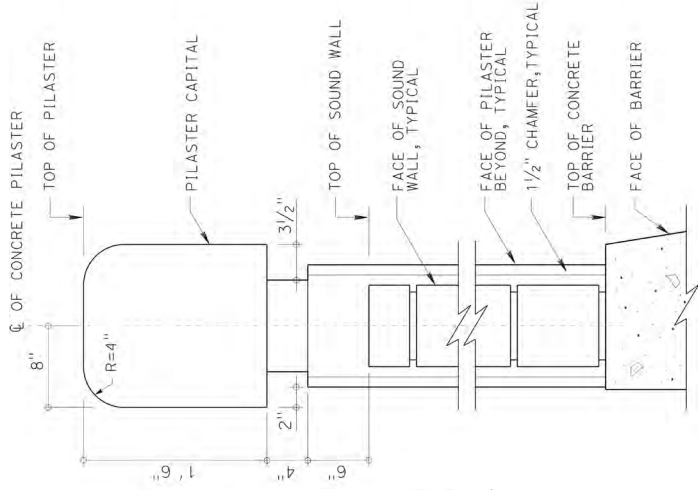
Sound walls for the I-605 corridor shall consist of integrally colored split face block in 3 different colors. Custom pilasters will be placed at regular intervals of 349'-4". The block motif is designed to incorporate placement of the pilaster. Sound wall heights will vary throughout the corridor and all design elements will adjust accordingly. Where appropriate, new sound walls will tie into existing sound walls. All pilasters shall be constructed vertical to profile grade (plumb).

NOMINAL SIZE	TEXTURE	COLOR
8" x 8" x 16"	SPLIT FACE 2 SIDES	NATURAL GRAY *
8" x 8" x 16"	SCORED SPLIT FACE 2 SIDES	OAK *
8" x 8" x 16"	SPLIT FACE 2 SIDES	PLACER CREEK *
4" x 8" x 16"	SPLIT FACE 2 SIDES CAP	NATURAL GRAY *

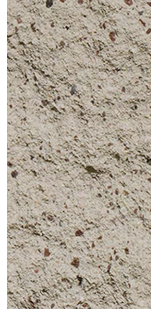
* TO MATCH ANGELUS BLOCK COLORS



TYPICAL PILASTER SECTIONS



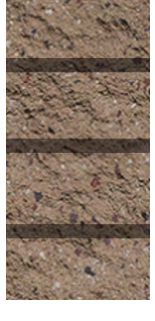
PERSPECTIVE VIEW



NATURAL GRAY

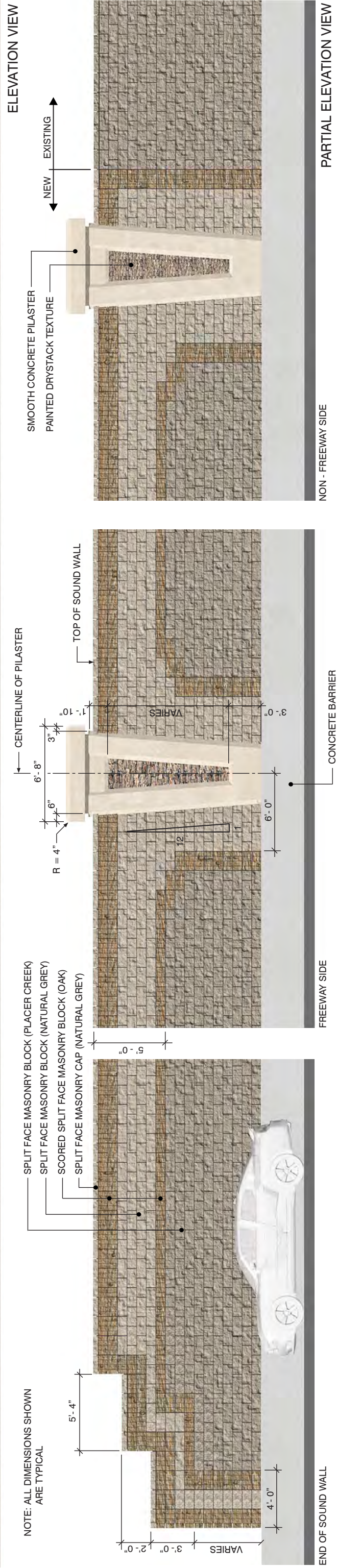


PLACER CREEK



OAK

349'-4" ON CENTER (TYPICAL PILASTER SPACING)



ELEVATION VIEW

PARTIAL ELEVATION VIEW

FINAL

REV 2 - JUNE 13, 2019

I - 605 CORRIDOR AESTHETIC MASTER PLAN

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5.0 LANDSCAPE

5.1 Plant Selection Process

Selecting plants for the I-605 Corridor was a long process which began years ago. Early freeway landscapes were a cornucopia of plants from all over the world. Large areas of freeway daisies and towering eucalyptus trees once dominated the roadside landscape. While these plants provide ample aesthetics and longevity, pests and diseases eventually took a toll on many of these non-native plants. In the ensuing years what has emergent is a recognition of the importance of native plants to sustain the roadside landscape. The work in selecting appropriate native plants continues as not all-natural plants are conducive to harsh roadside environments.

5.2 The Era of Regionally Appropriate Plants and Pollinators

In recent times the loss of habitat to urbanization has contributed to a decline in wild bees and butterfly populations. There has been a renewed interest in helping to sustain these pollinators populations. While the roadside landscape is a small fragmented habitat much can be done to improve the population of these pollinators. To help promote a roadside landscape that will attract to help the monarch butterflies, honeybees and hummingbirds, the new direction of freeway landscape design is to plant a pollinator-friendly roadside landscaping. This focus on pollinator-plant design is being done with regionally appropriate plants and native plants.

5.3 Plants as Design Elements

The plants selected for the I-605 Corridor will complement the new design of structures. Worker safety and long-term longevity of the plants is crucial to the success of roadside landscaping.

5.4 Ground Plane

To fill the ground plane low growing Prostrate Acacia was selected for drought tolerant, aesthetic appearance and ease of maintenance. When fully grown this plant will reduce worker exposure along the roadside.

5.5 Vertical Space

To accentuate the vertical space of the freeway several trees were selected, Western Redbud, California Sycamore, and Coast Live Oak. All the trees are California natives.

To add accent at critical points such as off-ramps or on-ramps, San Diego Red Bougainvillea and Spreading Yellow Lantana are used.

To lure beneficial insects to the roadside landscape Narrowleaf Milkweed and California Flannelbush will be planted in select areas.

5.6 Wall Screening

To screen walls and undesirable views a mass of Sugar Bush will add a rich, deep green background. Imperial Blue Plumbago will be used to accent the massing of Sugar Bush.

The Sound Walls will be aesthetically pleasing, and only minimal planting of vines will accent the wall features. No plantings of vines near pilasters, this is done to preserve their aesthetic character. Boston ivy will add a light greenery to wall and will be accented with the addition of Red Trumpet Vine.

5.7 Stormwater Requirements

Methods typically utilized to improve the quality of the water run-off adjacent project pavement includes detention ponds that allow pollutants to settle out and bioswales, which are earth ditches that use grass plantings along the swale to filter out the sediments and slow down water run-off.

Deer Grass and forbs are used in bioswales to give a distinct native character and along the perimeter showy Foothill Penstemon will accent.

California Sycamores, Bougainvillea, and California Flannelbush shall be utilized along the edge to provide aesthetic screening for the basins.

5.8 Drought Years

The greater Los Angeles area has a history of droughts. One drought occurred in the mid- 1970's by then the Los Angeles area freeways were lush with high water use plants. The freeway plant oasis was soon met with the reality of severe water restrictions. Caltrans District 7 recognized water use needed to be curtailed while keeping greenery along the freeway alive, motorist by then was accustomed to a sea of greenery along LA Basin freeways.

A team of Landscape Architects searched arboreta and consulted with horticulturist on plants that were drought tolerant. Many plants selected came from the continent of Australinea and the country of South Africa. These plants performed as expected and in the ensuing years reduced water consumption.

Regardless of the abundant rain received in the recent years Caltrans District 7 continues to be responsible with water usage

5.9 Early Recycled Water Use

Since the mid-1970's southern California region has endured three more droughts, the last one was the drought of 2012-2017. The Los Angeles area has seen a surge in population growth since the mid-1970 and the demand for scarce water supplies has grown as well. District-7 has recognized water is a precious commodity and since the 1970s has used recycled water for freeway landscaping when available. The quest to reduce water consumption for freeway landscapes continues to this day.

If recycled water opportunities arise during the design phase of the I-605 Corridor projects, recycled water will be used to the maximum extent possible, to reduce the need for potable water.

Trees



Cercis orbiculata
Redbud



Platanus racemosa
California Sycamore



Quercus agrifolia
Coast Live Oak



Acacia redolens 'Desert Carpet'
Prostate Acacia



Asclepis fascicularis
Narrowleaf Milkweed

Shrubs



Bougainvillea 'San Diego Red'
San Diego Red Bougainvillea



Fremontodendron californicum
California Flannelbush



Lantana 'Spreading Yellow'
Spreading Yellow Lantana



Penstemon heterophyllus
Foothill Penstemon



Plumbago auriculata 'Imperial Blue'
Imperial Blue

Shrubs



Rhus ovata
Sugar Bush



Salvia clevelandii
Cleveland Sage



Distictis buccinatoria
Red Trumpet Vine



Parthenocissus tricuspidata
Boston Ivy



Muhlenbergia rigens
Deer Grass

Ground cover

